

Tiverton Eastern Urban Extension

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DESIGN GUIDE



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1. Introduction

The Tiverton EUE Masterplan

Summary of Engagement Process

Assisting Masterplan SPD Conformity

Figure 1.01 The Masterplan SPD final masterplan layout



Legend

- | | |
|------------------------------|----------------------------------|
| A A361 | Residential |
| B Blundell's Road | Residential with rural character |
| C Post Hill | Employment |
| D Grand Western Canal | Employment (care home) |
| E Blundell's School | Mixed-use |
| F Tiverton Golf Club | School & sports pitch |
| G River Lowman | Landscape |
| H Als Brook | Fen, catchment & buffer zone GI |
| I Tidcombe Fen | Area B |

Urban interventions

- 1** Neighbourhood centre
- 2** Primary school
- 3** Community facilities and shops
- 4** Employment at the heart of the community
- 5** Employment uses
- 6** A new junction onto the A361
- 7** Residential development at a varied densities
- 8** A clear hierarchy of streets and spaces
- 9** Possible location for Energy from Waste plant
- 10** Possible location for gypsy / traveller pitches
- 11** Preferred vehicular link through NHS site
- 12** Potential non-vehicular link through Fairway

Landscape interventions

- 13** Pedestrian and cycle connections
- 14** The Sustrans cycle route
- 15** Green Infrastructure area
- 16** Wetland habitat areas
- 17** Woodland
- 18** Sports and recreation areas
- 19** Children's play areas
- 20** Green corridors and routes formed around retained hedgerows
- 21** Landscape community hubs - a focus for the multi-functional landscape
- 22** Landscape spine
- 23** Buffer to Post Hill properties
- 24** Neighbourhood allotments
- 25** Community orchards
- 26** Attenuation ponds throughout the area

800m from neighbourhood centre

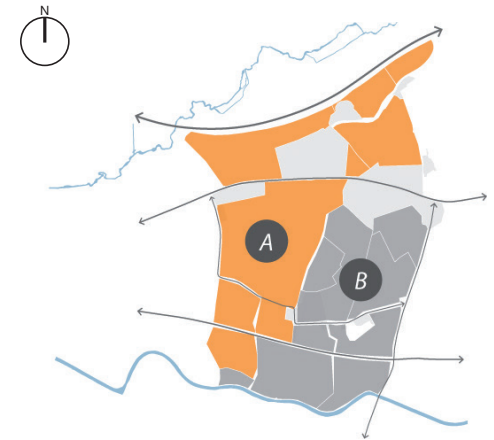


The Tiverton EUE Masterplan

The Tiverton Eastern Extension Masterplan work (final masterplan layout -adjacent left) was carried out in response to the allocation of land to the east of Tiverton in the Allocations and Infrastructure Development Plan Document. The masterplan is the result of approximately 3 years of joint working between Mid Devon District Council, landowners and land promoters and has involved extensive consultation.

The Masterplan Document was adopted as a Supplementary Planning Document in April 2014. The SPD sets out high level masterplanning principles for the whole Urban Extension but considers the area in two parts: A & B. The SPD includes more detailed analysis and proposals for part A with part B considered more conceptually. The SPD is visionary and pragmatic. It sets strong aspirations for a high quality, distinctive and attractive development. It also considers, with complex land ownership and interests, how the development of an urban extension which is coherent and not fragmented can be achieved in a phased way.

- Area A: Principal land holdings and NHS
- Area B: Land in multiple ownership



- 1 Tumbling Field
Old Mill Close
Leat Street
- 2 Oakfields
Moorhayes
- 3 Castle Street
Bampton Street
- 4 The Wolronds
- 5 Silverton
- 6 Landunvez Place
- 7 The Gables
- 8 Fir Close
- 9 Popham Close

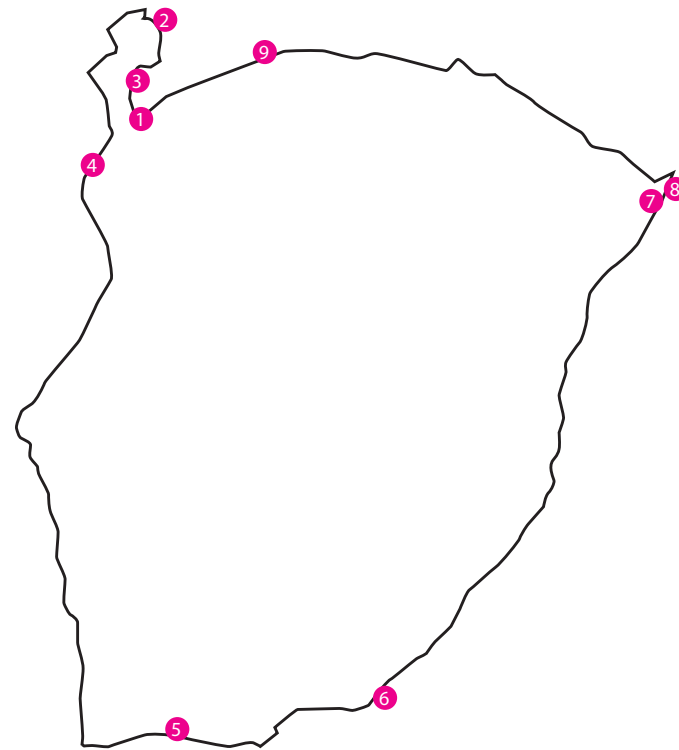


Figure 1.02 *The Stakeholder Bus Tour route visited a number of new and old developments around the district*

Summary of Engagement Process

The preparation of this Design Guide began by engaging with a variety of stakeholders in order that the emerging Design Guide material represents a consensus view and is rooted in the character and development pattern found in Tiverton and across the district. From early on the engagement process looked to gather information on a variety of housing developments nearby and assess their success. Those sites included are listed on the diagram to the left (page 8). Using this list and an appreciation of what made developments good and bad the Design Guide preparation could progress with careful reference to local precedents.

Before this, the Masterplan SPD had been informed by extensive public and stakeholder consultation. The masterplan principles were developed in the open with design taking place in workshop activities throughout the masterplan development.

The purpose of further stakeholder engagement during the preparation of the Design Guide therefore has not been to revisit the work achieved through the masterplan process but to acknowledge the conclusions and proposals the masterplan reached and build on these.

The engagement process has included a variety of participants: public representation, statutory consultees, developers, officers and members. The process has been structured around two key events with supplementary meetings with individuals as necessary to inform the ongoing work.

Event 1: Stakeholder Tour Day, 13th October 2015.

Stakeholders were invited to attend a tour of a variety of housing developments, new and old, in and around Tiverton. The list of

developments to be visited were gathered, prior to the event, from the stakeholders themselves with some thought given to whether they considered them to be good or bad examples and why. The list of locations visited can be found on the adjacent plan.

The tour stimulated significant discussion about the appropriateness of some approaches to development. There was initial hesitation as to why the day was not focussing on the site and understanding the masterplan. The intention of the day was to think beyond the site and existing proposals to consider how the design of the masterplan could be furthered through the consideration of local precedents. In this way the tour day also served a role informing the character of the area (chapter 2 of this document).



Figure 1.03 *The Design Guide engagement process included various events and groups of people*

Observations and comments from stakeholders on the various locations were recorded in the form of an assessment sheet completed for each location. Stakeholders were encouraged to comment with reference to the following headings:

1. A Legible Place
2. Public or private
3. A sociable place
4. A connected and accessible place
5. A special place

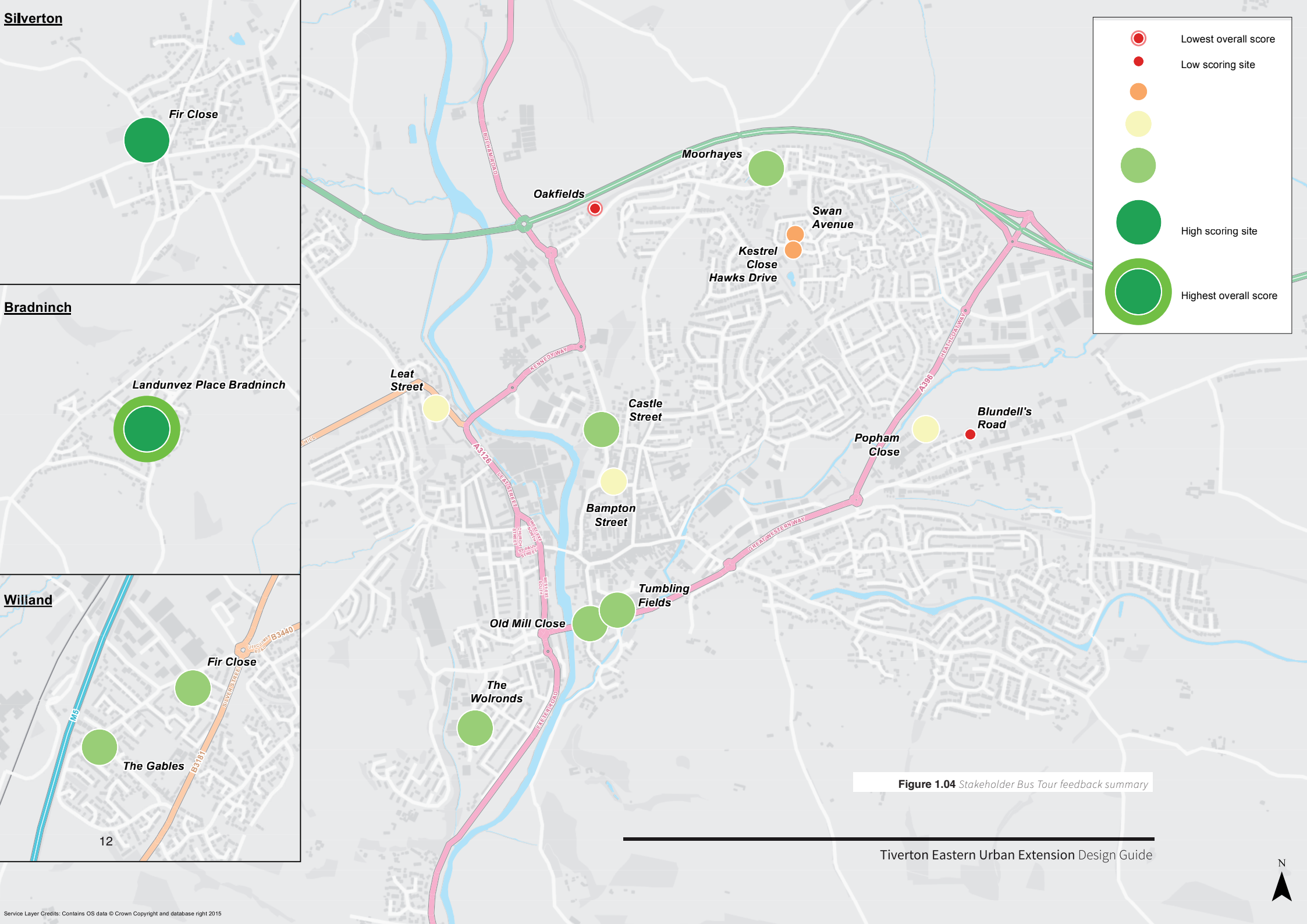
The headings, accompanied by a short explanation, also assisted those less familiar with recognised urban design analysis and provided a structure for the qualitative and functional appraisal of the locations.

Encouragingly a lot of feedback was gathered following in depth and informative discussion throughout the tour. Many of those at first hesitant about the purpose of the day concluded at the end that some very worthwhile contrast had been drawn between the different locations.

The tour provided some optimism that some very good quality and well thought about new development did exist locally. It also identified some very poor quality examples of development which was not desirable to replacate. In addition some of the older village and town environments demonstrated excellent models of development which were able to be considered in the light of the new developments in order to identify more precisely what it is that makes them successful and attractive.

All of the feedback gathered at the event provides the foundation

of the character study found in chapter 2. The individual appraisal sheets completed during the day were processed with the adjacent map (page 12) summarising the locations visited. It indicates the quantity of feedback received in each location (size of circle) coupled with the overall impression of the development quality (colour of circle).



While various general themes could be identified throughout the day as to what made a good development, the lessons learned can be summarised simply as the following:

- A successful place is the cumulative effect of a range of factors;
- Not 'wrong typology' but 'wrong context' -similar elements of the design could be identified as successful in one place where as failed in another;
- The quality of the construction and materials is very obvious.

Event 2: Workshop Day, 17th November 2015.

The second event in the engagement process included 3 workshop meetings addressing the following key groups of people:

- Stakeholders (public representatives and statutory consultees)
- Developers
- Officers & Members

The aim of the event was to explain the emerging structure of the Design Guide. Presentations were made to all 3 groups which explained the approach:

- To build on the masterplan as a structural foundation (primarily the open space hierarchy and the defined character areas);
- To categorise a series of character areas, residential and others;
- To consider all elements of the Design Guidance in the context of Centre or Edge locations within the masterplan structure.

In addition to this, the stakeholder group had a summary discussion of the tour day and were tasked with applying the knowledge of housing precedents and lessons learned to the emerging residential character areas.

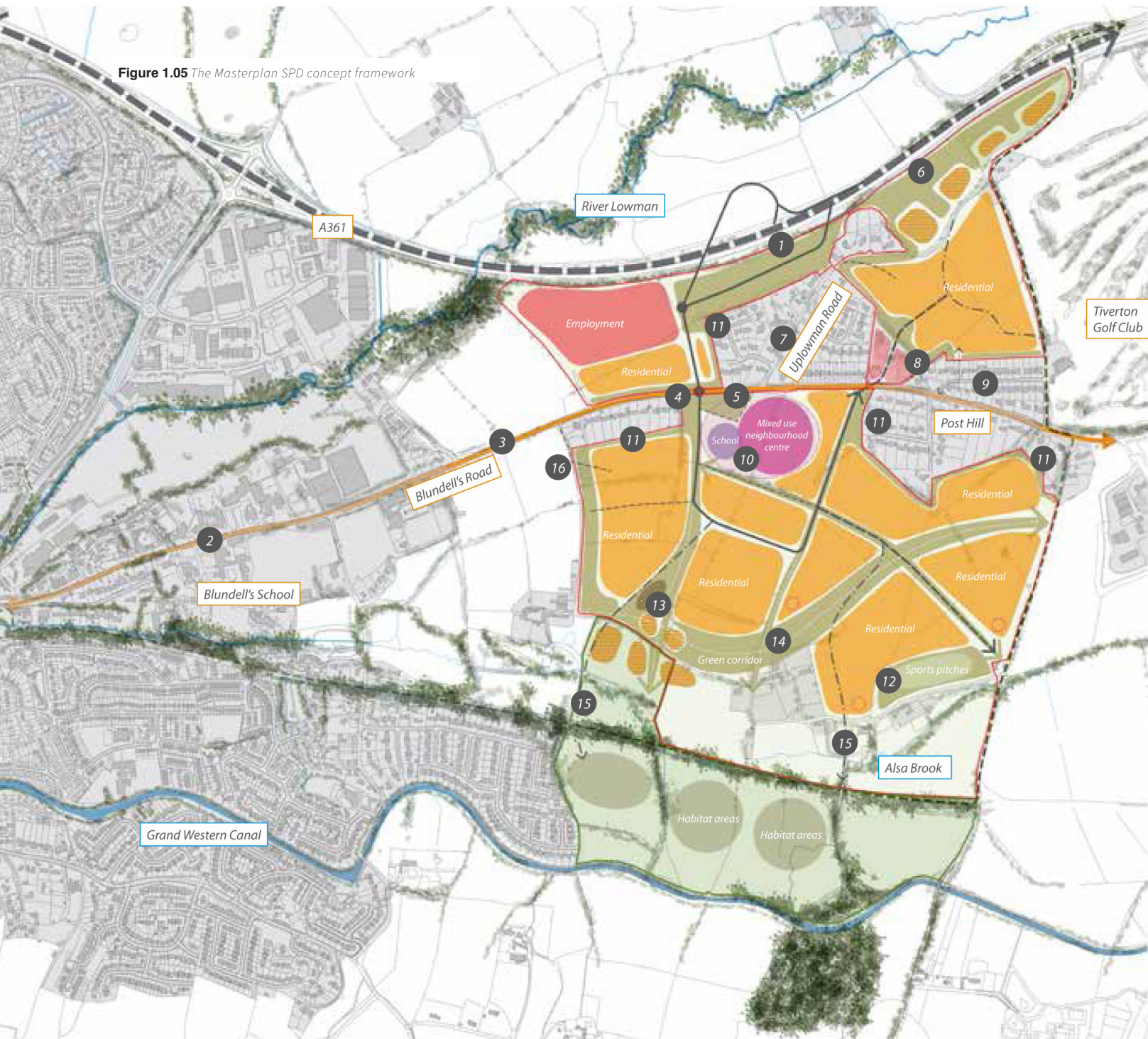
A message of design aspiration and collaborative working was communicated to the developer group -the Design Guide being described as a facilitative and labour saving instrument in this context. Developers were asked to consider any themes they felt were obstructive to achieving good quality design through

the planning process. This was in order that the Design Guide be steered towards facilitating effective and clear pre-application engagement.

The Officers and Members discussed the use and application of the document within the development control process including the consideration of how close association with the Masterplan SPD and engagement in the design review process could provide additional mechanisms for the robustness of the Design Guide. The proposal for a pocket summary of the Guide was tabled by the design team, useful as a table top reference and checklist for officers as well as developers and their design teams -the focus being on creating a Design Guide which served as a shared asset to both parties.

The result is to form a robust Design Guide which, having been through early engagement with those who will use it, provides meaningful and tested guidance for use in the preparation of the reserved matters applications. Further more the process of applying the design guide in the pre-app process will have been rehearsed and the material refined to suit its use and users.

Figure 1.05 The Masterplan SPD concept framework



Legend

- 1 New junction onto the A361
- 2 Traffic calming at Blundell's School
- 3 Environmental enhancements along Blundell's Road
- 4 New junction on Blundell's Road linking to A361
- 5 Shared space and village green at neighbourhood centre
- 6 Open space with attenuation ponds
- 7 Highway improvements to Uplowman Road
- 8 Possible link to north eastern area
- 9 Possible link onto Fairway
- 10 Mixed use neighbourhood centre including school, shops and employment space
- 11 Green buffer to existing properties
- 12 Sports pitches
- 13 Allotments
- 14 Greenspace on steep ground
- 15 Links to southern area of Green Infrastructure
- 16 Possible secondary link to Blundell's Road

- Eastern Urban Extension Area AL/TIV/1-7
- Green Infrastructure Area AL/TIV/3GI
- Employment
- Employment (care home)
- Satellite Employment
- Residential
- Residential with rural character
- Education
- Open space and landscape
- Mixed use centre
- Playing fields
- Existing development
- Primary route
- Secondary routes
- Tertiary
- Other routes and connections

Assisting Masterplan Conformity

As an adopted policy document, the Masterplan SPD represents the quantitative framework for the development of the Tiverton Urban Extension. The Design Guide sits within this framework providing a range of solutions which will ensure the quality with which these objectives are achieved.

Through the appropriate engagement with relevant parties and exposure to a critical stakeholder and user engagement the Guide achieves a level of technical rigour to ensure it conforms to all of the principles of the Masterplan SPD and is applicable to emerging reserved matters applications. It has also been subjected to a careful policy compliance process to ensure it becomes a reliable document in the development management process. The document has been conceived as usable and clear in the development management process such that it can provide a useful backdrop to the design conversation.

The Design Guide defines the aspirations of the Masterplan SPD and the document structure reflects the primary elements of the masterplan's vision for the Eastern Urban Extension and by doing so demonstrates how these can be delivered

The Masterplan SPD Vision describes:

- A structured framework
- A Green Place
- A new residential community
- A modern lifestyle
- A place to be proud of

The Design Guide brings forward these Vision elements as the focus of each chapter. It seeks to clearly communicate how those submitting reserved matters applications can do so while realising the very highest level objectives for the Eastern Urban Extension.

The Masterplan SPD is an adopted document which outlines the requirements of any reserved matters applications. Most notably any reserved matters application must be accompanied by an Urban Design and Architectural Principles document. The Design Guide serves as a tool and a labour saving device to satisfying these requirements. It is not placed as an additional hurdle to the formation of reserved matters applications. Moreover it has been prepared to avoid any hurdles to achieving the objectives of the Masterplan SPD.

2. Character

Place

Architecture

Landscape

Topography & Gradients

1. Castle Street, Tiverton. Varied public realm including distinctive central drainage channel. Buildings vary but include long terraces, proximate to back of pavement.
2. Landunvez Place, Bradninch. Continuity of buildings with good quality detailing and materials.
3. Silverton. Long terraces but including varied and historic materials. Street width very narrow in places.
4. Halberton. Street-facing residential development.
5. Abode, Cambridge. Sensitive contemporary development and use of the street as a neighbourhood space as well as access.



Place

The development of this Design Guide has involved a distillation of the Masterplan SPD's identification of Tiverton's local characteristics. In addition to this, further investigation has been conducted into the physical attributes of the locality.

These investigations have been based on the site's geographical features and also the context of recent and historic local development in order to understand the existing sense of place and how best to guide the creation of a sense of place for the EUE. This process was undertaken together with groups of key stakeholders.

Engagement with stakeholders has been important in the development of this Design Guide, including local residents and community representatives. Good place making creates good communities which are defined by their citizens.

The spatial design of Tiverton and the town's surrounding context is an important point of reference for this Design Guide, aiding in the grounding of the development to its locality.

The character study informing this Design Guide has considered a wide range of nearby settlements, as well as the various places visited as part of the Stakeholder Tour day, in order to understand the typical settlement form across the district.

The examples of nearby settlements shown overleaf all demonstrate a linear form to their centres despite the overall form or size of the settlement.

This has been an important consideration in the preparation of design guidance for the Neighbourhood Centre but also in defining the design principles which govern the Urban Extension as a whole.

The Blundells Road corridor serves a similar function to many of the primary routes through the settlements which form part of the character study. Along this route the relationship that existing development, such as Blundells school, will have with the road

Bradninch



Thorverton



Silverton



South Molton



Cullompton



Figure 2.06 Existing Mid Devon settlement spatial analysis



and the public realm is also similar to the study settlements. In summary these linear centres to many of the nearby places demonstrate the following qualities and characteristics which are to be reflected in the design of the Urban Extension:

- The main historic route through the settlement is the location of the highest mix of uses
- This focus of mixed uses extends in a linear pattern, not concentrically creating activity along the length of the street
- This requires that the public realm functions as a linear open space with the capacity to accommodate a variety of activity associated with different building uses
- Most buildings vary in use vertically but not horizontally meaning the length of the ground floor elevation is the same use (mainly retail) with floors above used for offices or residential
- All buildings front onto the street
- The street width varies to create variations in the public

realm and opportunities for significant scale linear open space

- Variation in building height corresponds to the need to create landmarks -either as a function of their use, or their position eg. at the corner of a side street
- Although in places there are long continuous rows of terraced buildings, routes off of the main street connecting to the surrounding settlement area are frequent (approximately every 40m) and vary in scale -footways and vehicular routes
- Tree planting and other street furniture help to define the linear form as an open space
- There are generally no large areas of car parking, instead either kerbside parking or small groups of eschelon or end-on parking.

The Design Guide offers a methodology based on a linear form development at the neighbourhood centre with 'perimeter blocks' forming the structure of the development beyond (see p.32).

Irregular or informal blocks, with more variety of angles, will give the impression of a townscape that has evolved over time, perhaps with a village, rather than urban or town character. Informal layouts will require more care and a flexible approach to house design to avoid disjointed and poorly enclosed streetscape. However, larger sites or urban extensions will require a combination of the formal and informal arrangement to create distinct character areas.

'Perimeter blocks' allow for a clear and legible hierarchy of streets and spaces, whilst allowing a characterful place to be delivered. Different approaches to the form of the 'perimeter block', its overall shape, boundaries and building lines will vary in response to the different character areas.

Overall, the 'perimeter block' allows the structure and character areas of the Eastern Urban Extension to be established. In so doing, a seamless form to the development can unfold across the variety of land ownerships and land parcels. Detail, including architecture, can help to reinforce structure.

1. Local agricultural vernacular at development edge
2. Bay projections add interest to the building elevation
3. Considered glazing proportions
4. Grander urban terrace with generous frontage
5. Late 20th Century mid-density development
6. Contemporary house types with natural boundary treatment
7. Repeating terrace house type with bay windows
8. Successful recent development



Architecture

The architecture of place is a key contributor to the identity of place.

Reciprocally, architecture also identifies place by responding to the characteristics of its location; the topography, climate, orientation and the availability of materials. These factors and more inform an architectural response, from the formation of shelter and protection to the presentation and expression of use, identity and landmarking.

There are various architectural attributes that have been identified in Tiverton and the surrounding area, both contemporary and traditional that can positively contribute towards the place-making of the EUE.

Important architectural attributes include:

- Ochre to red multi stock brickwork with a contrasting mortar.
- Stone rural buildings and boundary walls.
- Red Halberton stone.
- Render with a varying neutral painted finish.
- Roof planes that project beyond the wall.
- Increasing window proportions through time.
- A Sensitivity towards proportion
- Parapet frontages on significant buildings
- Bay fronted terraced houses.

Some landmark buildings are built of alternate yet complimentary materials such as the stone built Town Hall. This is an effective tool for articulating emphasis in the built realm.

There are also characteristics of architecture that are not specific to Tiverton that can contribute positively to a sense of place-making.

1. Mature landscape elements provide established character but also represent sensitive ecological assets.
2. Existing local lanes and places have a strong sense of landscape setting
3. Existing landscape features provide a good framework for the new development.
4. The wider area of the east of Tiverton is a rich landscape of undulating Devon countryside.
5. Some existing buildings in the landscape demonstrate a compatible form and scale.
6. North facing slopes with significant gradients but presenting the potential of wide reaching views and distinctive architecture.



Landscape

East Tiverton is unique because of its outstanding natural assets.
- Tiverton EUE Masterplan SPD Vision.

The sense of place associated with the Tiverton EUE is significantly defined by its setting in the landscape, from the hedge-rowed natural boundaries to the distant views across the countryside.

The Design Guide endeavours to ensure that the features of the landscape will be emphasised wherever possible and therefore aid in defining the EUE's sense of place.

These principles of using the landscape to tie a development to its locality are central to the key principles of Centre to Edge identified in this Design Guide.

Some of the natural features that inform this Design Guide include:

- Topography varying significantly across the site.
- Existing natural site boundaries - hedgerows, mature planting etc.
- Vistas towards the north from the North Eastern Area and NHS sites
- Vistas to the south from Blundell's Road / Post Hill and throughout the site
- A sensitive edge treatment between development and the landscape

The process of developing this guidance involves the application of built development typologies to respond to the landscape, emphasising existing view corridors and considering the suitability of housing typologies and locations for infrastructure to best harness local distinctiveness and create a sense of place.



-  1:5 - very steep
-  1:10
-  1:15
-  1:20
-  Less than 1:20

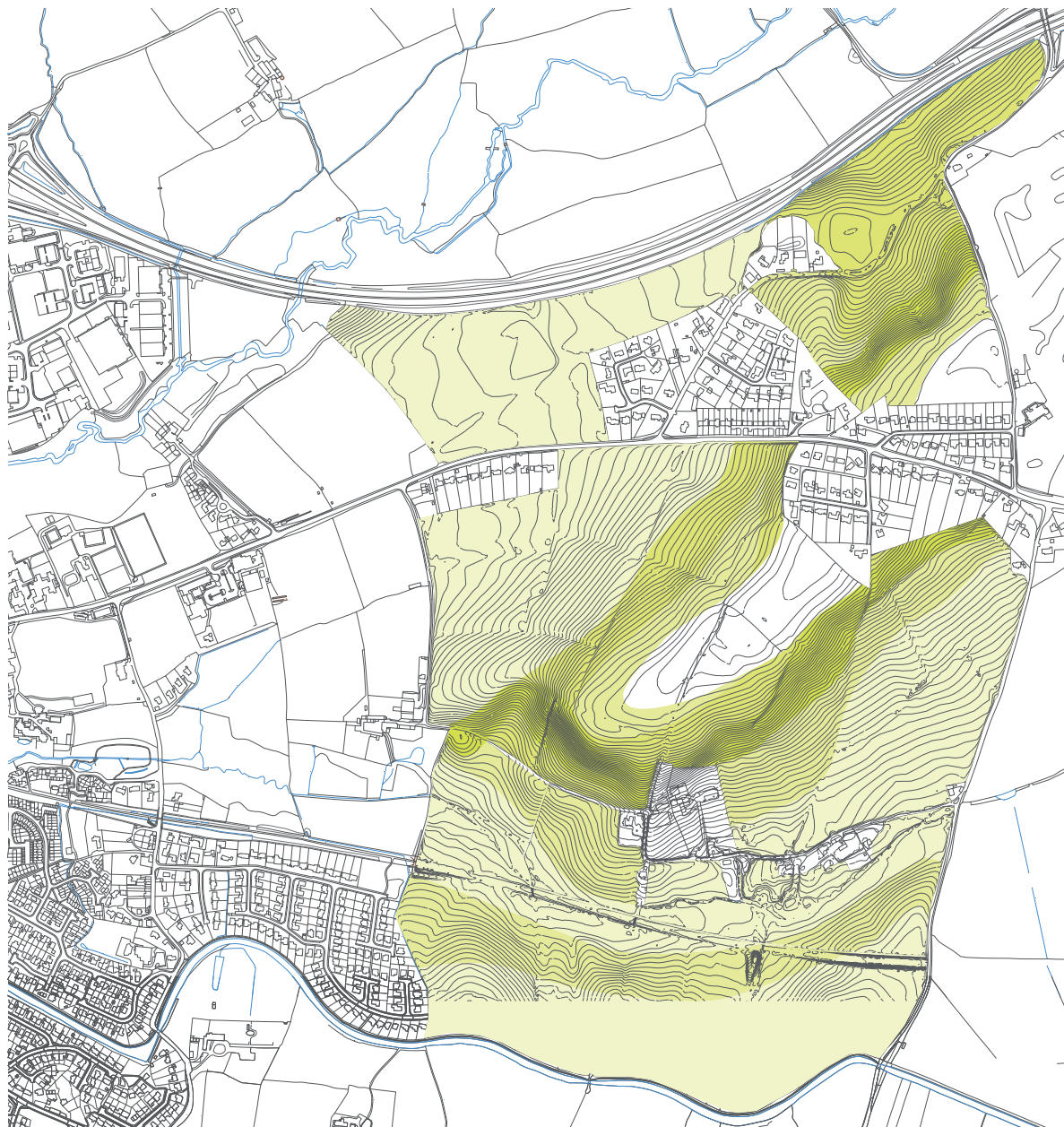


Figure 3.07 Existing Topography & Gradients

Topography & Gradients

The topography and gradient analysis carried out in the Masterplan SPD and shown over leaf has had a significant influence on the layout of the Masterplan SPD. Through the reserved matters stages these constraints and characteristics of the site should continue to influence the design and contribute positively to the contrasting character across the Urban Extension.

Considered in the context of the Centre and the Edge of the development the topography and gradients provide different opportunities to provide the foundation for views and sightlines throughout the development and to establish the framework for a visual and legible relationship with the surrounding countryside.

Design at the detail level will need to balance the operational and functional requirements of streets and development blocks with the overall aspirations of creating an Urban Extension which is structured around a legible relationship between Centre and Edge.

Where necessary building form can vary to suit the topography. In some cases the Masterplan SPD indicates development is laid out to take advantage of level streets along the contours. In other circumstances, in order to make the necessary connections or form important visual links development crosses the contours. Each will result in contrasting building form which will distinguish one area of the development from another.

The majority of routes shown in the Masterplan SPD and in subsequent structuring guidance in this document achieve adoptable gradients of up to 1:12. In some situations steeper gradients are necessary and initial consultations with Devon County Highways confirm that the alignment of these routes which result in steeper gradients of up to 1:8 for short distances are acceptable where other measures are used to ensure safety and usability -lead-ins to junctions should always achieve 1:12 gradient; no side roads should junction onto to sections of road which exceed 1:12; isolated areas of regrading of the topography might be appropriate in order to mitigate steeper sections of road; care should be taken to avoid overly engineered public realm or building form along steeper sections of road.

3. Design Guidance

Using This Design Guide

- a Creating a Green Place
- b Establishing a Structure
- c Creating a New Residential Community
- d Creating a Modern Lifestyle
- e Design Detailing

Using This Design Guide

This Design Guide has been prepared in the open with reference to a clear stakeholder engagement programme. The Design Team have taken seriously all of the comments and feedback received through this process to refine a Design Guide which is in the first instance a true reflection of the Masterplan SPD structure and design objectives.

In part this process of engagement has rehearsed the effectiveness and relevance of the Design Guide, adding weight to its use within the pre-application process. Because of this, the Design Guide takes a form which is clear and concise, conveying the fundamentals considered essential to achieving the Masterplan SPD aims.

Subsequently, the intention is that this Guide is used collaboratively between the Local Authority, its partners and applicants submitting reserved matters applications. It is prepared in order to bring clarity to the design requirements and expectations embodied (but elaborated on) through the Masterplan SPD.

It is intended as a tool for the Development Management team and will be a material consideration through the formal application process but is also perceived as a shared asset for applicants such that it creates a meeting place for conversations regarding design throughout the pre-app process.

The Design Guide does not focus on the control of every detail of the design. It does identify the fundamentals of the design which are believed to be critical to achieving the aims and objectives of the Masterplan SPD and outlined some approaches which guide design towards achieving these. It is important therefore that there is early engagement with Design Guide through the pre-app process for applicants, landowners and their design team.

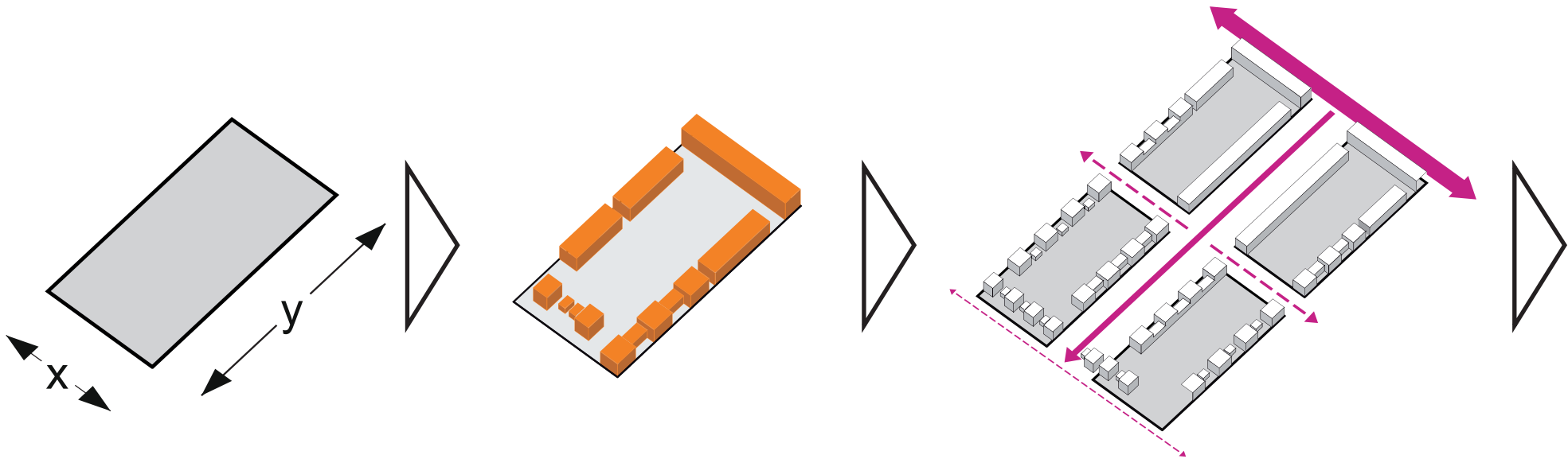
As a complimentary tool, this Guide is accompanied by The Pocket Summary Guide for desktop use and checklisting. This is a single page fold out summary of the entire Guide contents. This is provided to enable table top use of the Guide within the Design Enquiry Service and more widely in pre-application discussions.

It can also be used to share the Design Guide content in a cost effective and easy to access way among applicants, potential interested parties and their design teams.

In parallel to the pre-application process, and in accordance with national policy, this Design Guide advocates the use of the design review process to further galvanise design quality. The Design Guide has been the subject of a briefing to the Devon & Somerset Design Review Panel and applicants are strongly encouraged to engage with the panel at 3 separate sessions. The intention is for the Panel to offer a constructive and independent review, measured against the Design Guide -see also a more detailed guide to the recommended Design Review engagement process found in chapter 3, part e.

Design of the Tiverton Eastern Urban Extension should follow a sequence of structure first and detail later. Applicants are encouraged to use the following methodology:

NB Rectangular blocks are shown for ease of illustration.



1.

Block size and shape

This determines the permeability and legibility of the development.

2.

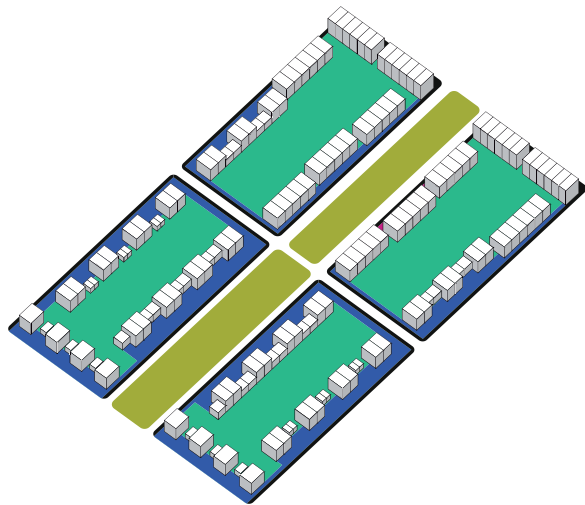
Divide the Block

Arrange the buildings around the perimeter according to character areas. The Design Guide provides a series of non-site specific example block plans for each character area which should be used to inform the site specific response in each character area.

3.

Street Hierarchy

Combine blocks in a layout to create a legible street hierarchy. It is important to consider which edges of the blocks form which streets.



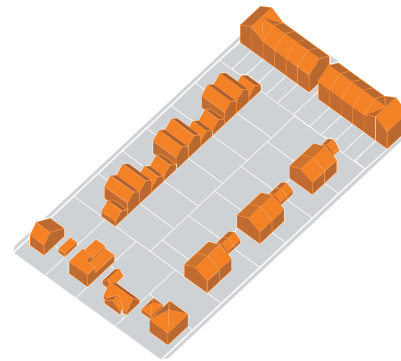
4.

Public and Private Space

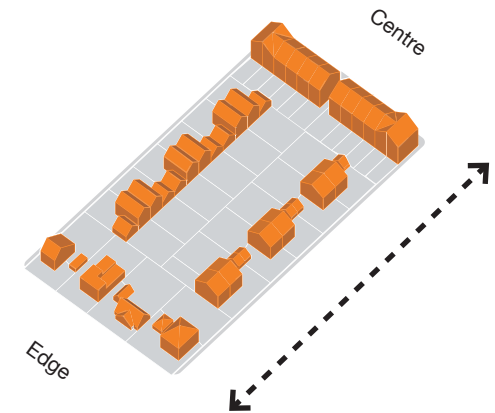
Different approaches to front and rear gardens, boundaries, parking and the public realm are suitable in different character areas.



5.



6.



Centre & Edge

Overall, structurally and through the detail, there should be a distinct sense of the Centre and the Edge.

Creating a Green Place

- a1 A Green Network
- a2 A Garden Neighbourhood
- a3 Centre to Edge
- a4 Edge to Centre



Figure 3.08 Masterplan SPD network of green spaces

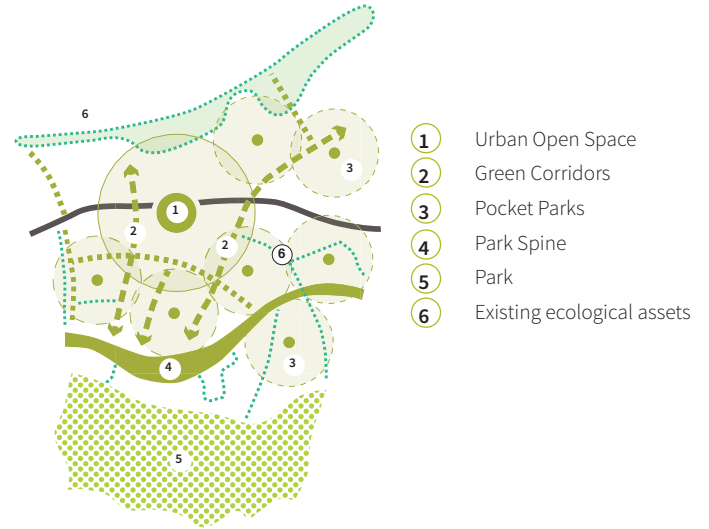


Figure 3.09 Green Infrastructure Hierarchy

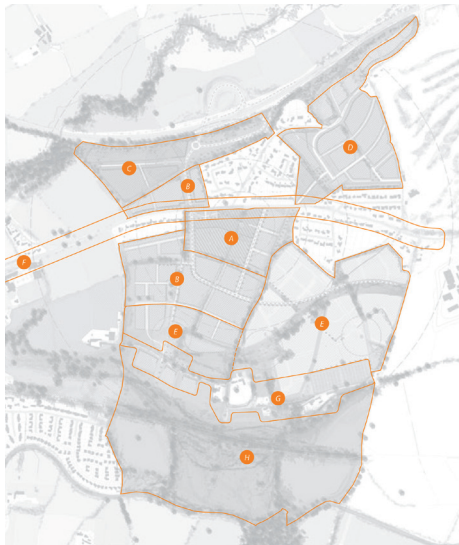


Figure 3.10 Masterplan SPD character areas

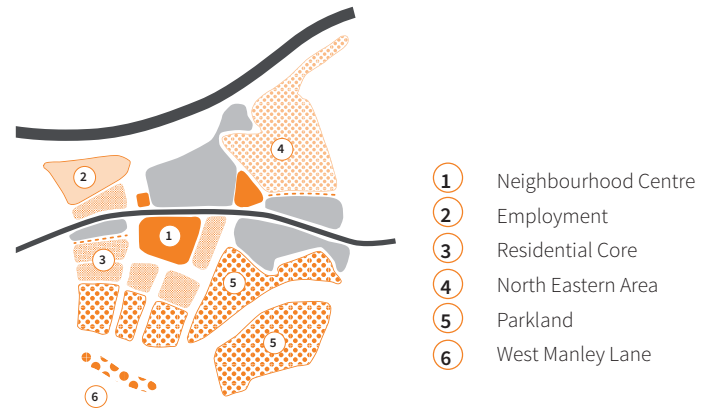


Figure 3.11 Design Guide categorisation of character areas



a1

A Green Network

The Masterplan SPD establishes a very clear structure to the development as a whole. While the area is a patchwork of landownership and delivery may occur across a number of complex phases, reliant on interrelationships between landowners, developers and agencies, the Masterplan SPD and this Design Guide together provide a coherent strategy to create a distinct settlement areas.

This is achieved by adhering to the structure imposed by the existing landscape, considering the topographical constraints and upholding the qualities and characteristics of the rich landscape setting to the east of the town.

Subsequently the structure of the resulting development pattern is informed primarily by this green network. The Masterplan SPD describes a series of character areas to the development which work with the green network to define development parcels and block structure suitable to the positioning within the Masterplan. Furthermore proposals for new green links and spaces enhance the existing ecological and landscape assets and establish a strategic green grid framework to the development.

From this the Design Guide derives the structure necessary to realise the vision of the Tiverton Eastern Urban Extension. It categorises a clear open space hierarchy (as defined in Fig. 3.09), including new and existing open space and landscape typologies. The Green Infrastructure within this hierarchy will be expected to

be structured such that it will appear seamless across the Eastern Urban Extension and across the development parcels. It will take a formal approach at the Centre of the development becoming informal towards the Edge. Detailed development proposals will be expected to show how these proposals are articulated through the design. In so doing, it will encourage healthy lifestyles helping to connect people to the locality - visually and emotionally, through a network of safe, green, clearly defined sustainable routes. Subservient within this structure, the Design Guide establishes a form to the built development including the residential character areas, the employment area and the Centre and Edge conditions.



In defining the principles for the Garden City in his 1902 publication 'Garden Cities of Tomorrow' Howard identified the attractions of both the Town and the Country.

A Garden Neighbourhood

The Masterplan SPD proposes that the urban extension becomes a new Garden Neighbourhood for Tiverton. This concept makes reference to the rich landscape setting to the east of the town which includes numerous public and ecological green space assets and is set within the wider context of the Devon countryside and outstanding landscape.

The concept of a Garden Neighbourhood goes back further though to the emergence of the Garden City movement at the turn of the twentieth century. Planner, Ebenezer Howard in the publication 'Garden Cities of Tomorrow' described the attractions of both the town and the countryside and using the analogy of magnets identified that people were drawn to both. This created a tension between the two. In resolving this tension he described the Garden City, a place where both town and country could be enjoyed. The concept of the Garden City

brought together the necessary mix of uses and critical mass at its centre in order that the settlement remained autonomous from other settlements nearby. This centre was necessarily dense in order to maintain the compactness of the town. This in turn allowed for less dense development at the outskirts set within a diverse landscape context but never too far from the services and employment critical to sustaining the town and its people. In this way Howard described how the Garden City could offer the best of both worlds: the town, in the countryside.

Following the success of early prototypes of the Garden City model, attempts were made to translate the principles to other contexts. The idea of the Garden Suburb appeared and now, since the introduction of the National Planning Policy Framework in 2011, the 21st Century interpretation, the Garden Neighbourhood, has emerged.

Unlike the Garden City, the Tiverton Urban Extension does not propose to be autonomous from Tiverton, in fact the creation of the Urban Extension is founded in the strategy to maintain Tiverton as a sustainable settlement in its own right. The relationship and interdependence between the urban extension and the existing town are very important.

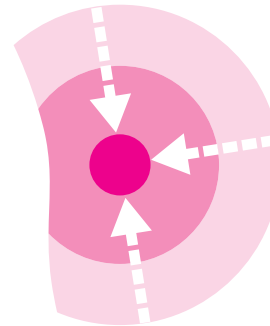
The Urban Extension is large enough to require its own neighbourhood centre and the Masterplan SPD proposes a mix of uses at the centre, including a small shop, a primary school and other community facilities. The Urban Extension is also set within a rich landscape and at the edge of the town with views and access to surrounding countryside and good quality open space. For these reasons the attractions of the town and the country which appealed to Howard as he conceived the Garden City model are pertinent here too. Moreover, to ensure the

conservation and prevalence of the surrounding landscape and wider countryside it is imperative that the contrast between the centre and the edge be very legible.

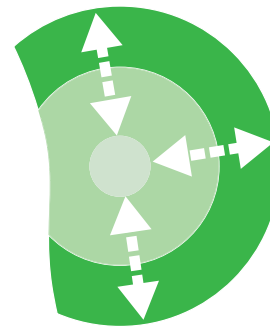
The principle of the Garden Neighbourhood goes beyond an aesthetic quality applied at the plot and house scale. It requires a strategic and structural approach to the layout of the whole of the urban extension.

This design guide is itself aimed primarily at achieving a strong contrast between the centre where a mix of uses and urban character can be enjoyed, and the edge where a diverse landscape and rural character can be experienced with essential links and connectivity between the two so that at any one time the people are able to resolve the tension of whether to choose the town or the country just as Howard had intended.

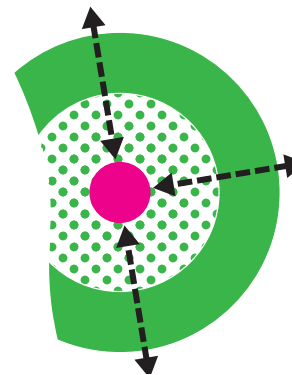
THE TIVERTON EASTERN URBAN EXTENSION:



looks in towards a focus of higher density and a mix of uses in one place...



...but **looks out**, as the landscape reaches in, to meet with the rich and diverse landscape context...



...to create a very legible sense of **Centre and Edge**.

Figure 3.12 *Centre and Edge principles*

a3

Centre to Edge

The centre should be of its own character but should also have a connection with the development edge. The following attributes characterise this:

- Higher density civic development
- Green Boulevards with public realm occupation.
- Clear connections through the residential development and open spaces.
- Views capturing nearby natural landscapes



Figure 3.13 Centre to Edge - Illustrative collage

a4

Edge to Centre

The edge should be of its own character but should also have a connection with the development centre. The following attributes characterise this:

- Low density residential development feathering into the landscape.
- Regular permeability for pedestrians to the nature beyond.
- Green Boulevards with opportunities for occupation.
- Clear connections through the residential development and towards the Neighbourhood Centre

Detached dwellings to the edge of the development.

Green boulevards with higher density residential development identify principles routes through site.

Buildings disperse at the edge as the surrounding landscape reaches into the development.

The centre is visible as a legible destination from around the development.



Figure 3.14 Edge to Centre - Illustrative collage

Establishing the Structure of the Place

b1 Neighbourhood Centre

b2 The NHS Site

b3 Employment

b4 The Blundell's Road Corridor


b5 The Western Edge Relationship

b6 The Northern Edge Relationship

b7 Streets: Centre to Edge & Green Boulevards

b8 The Green Infrastructure Hierarchy

- b1** Neighbourhood Centre
- b2** The NHS Site
- b3** Employment
- b4** Blundell's Road
- b5** The Western Edge
- b6** The Northern Edge

 Southern and Eastern edge treatments are addressed in the relevant character area chapters

Establishing the Structure of the Place

The characteristics of the various development areas of the Urban Extension are informed by the formulation of a clear structure to development, defined by the principles of Centre and Edge.

In parallel to this, the definition of these character areas is informed by existing physical features of the development such as Blundell's Road and the edge condition with the A361 and important elements of the Masterplan SPD such as the Neighbourhood Centre and the NHS Site.



Figure 3.15 Establishing Structure masterplan

b1

The Neighbourhood Centre

The Mid Devon Local Plan part 2, policy AL/TIV/4 requires as part of the Urban Extension, the provision of a 1.9 hectare primary school site and a 2 hectare area as a Neighbourhood Centre which should include retail and community buildings. The Masterplan SPD designates the location of this Neighbourhood Centre as focused on the Blundells Road corridor. It also outlines how the quantum of space can be accommodated in this location.

This Design Guide provides information to guide the formation of the Neighbourhood Centre and indicates how the buildings and spaces may be arranged to achieve a high quality and attractive place in the Urban Extension and in the context of the wider town and landscape setting. Figure 3.18 has considered how

the required quantum of retail, community and primary school provision can be accommodated. An alternative layout may also achieve the objectives. It also addresses how the Neighbourhood Centre can achieve a form which is recognisable as the Centre of the Urban Extension whilst being legible and accessible to all residents of the area.

An Urban Open Space

The design of the Neighbourhood Centre encourages interaction between people, ease of access to facilities and an enjoyable experience at the heart of the Urban Extension. The urban open space helps establish a linear form to the Neighbourhood Centre reflective of the some of the settlement centres identified in the

character study. Located along the Blundells Road corridor, with buildings set back around its perimeter, the urban open space identifies the centre and should present a high quality image at the heart of the urban extension. The space should be designed to allow for flexible use for a range of activities and the spilling out of indoor activities from the surrounding buildings - cafe, school and shops.

Shared Space:

The design of the urban open space at the Neighbourhood Centre should incorporate the treatment of the highway along Blundell's Road. Creating a shared environment that prioritises pedestrians and cyclists over vehicles is required. Encouraging individuals to



Figure 3.16 *Key Area Plan*



Buildings with a mix of uses fronting the open space. Shops, services and facilities at the ground floor with residential, office or community use above.

The school building and main entrance create a prominent feature. The entrance opens out onto a safe vehicle free open space. The main way of getting to school is by foot or by bike. Any staff car parking is provided to the rear of the building.

Buildings around the central open space are generally 2.5 storeys, creating a strong edge and enclosure of the open space. In the case of the school building, efforts should be made to increase the height by introducing office or community space above or by the arrangement of the internal volume of the building

Residential buildings front the open space with windows and doors overlooking the centre.

Blundells Road remains a primary route through the area but is designed as a shared space with some car parking where traffic speeds are low and pedestrian movement is safe.

The central open space is a simple landscaped space, encouraging a number of activities, vehicle free and surrounded by overlooking buildings and trees

Figure 3.17 Life at the Neighbourhood Centre

walk or cycle, through the use of level crossings and paths that follow direct desire lines, whilst maintaining Blundell's Road for through traffic, is sought. In so doing, connectivity across Blundell's Road for existing and new residents will be enhanced allowing easy access to the neighbourhood centre and beyond. Bus stopping areas should be included in the design of the space and located to facilitate easy and direct access between the mode of public transport, the centre and the existing and new residential areas. Any innovative scheme involving Blundell's Road will undergo a detailed highway design and safety audit as contained in the Quality Audit (TAL 5/11).

The new junction to the west of the Urban Extension on to Blundells Road will provide an important new link into the area and into Tiverton when approaching via the new junction on the A361. The treatment of this junction and the design of the built form adjacent to this junction will form an important gateway to the town and to the development. It's proximity to the Neighbourhood Centre also means the junction needs to serve an important public realm function. It is proposed therefore to use a shared surface model as the basis of this junction design which seeks to optimise traffic movement but in a speed-reduced environment. The performance and environmental objectives of this junction can be achieved using a roundabout layout with a level surface and high quality materials. This model has been tested and confirmed by Devon County Highways for capacity and function.

Permeability to the Centre

The Neighbourhood Centre is positioned centrally within the overall masterplan for the Urban Extension to ensure it is within easy reach of the majority of the development. This can only be achieved where connectivity to the centre is also achieved. The surrounding block form should create direct routes to the centre and form a simple, understandable structure to the development which creates a highly permeable framework for pedestrian movement.

Prominent Green Corridor

The Masterplan SPD is fundamentally structured around a framework of Green Infrastructure. As part of a hierarchy a series of green boulevards extend from the Centre to the Edge of the urban extension to create legibility and prominence to both. Where these green boulevards reach the Neighbourhood Centre they should be highly prominent and celebrated and incorporated into the design of the public realm and infrastructure at the centre.

People and Activity, Not Vehicles and Circulation

The design of the Neighbourhood centre should prioritise people in its layout and scale. The combination of streets and spaces create a generous public realm. The arrangement of buildings establishes a strong sense of the direction to the centre and with the permeability of the surrounding block form facilitates easy navigation to and from the centre. Parking is provided at the

Neighbourhood Centre but the first consideration is the activity of people and the movement of pedestrians and cyclists to and from the Centre from the whole of the Urban Extension. For this reason, shops, community buildings, offices and the school are orientated to face the streets and spaces which they are adjacent to. This creates desire lines to and from buildings which results in increased activity in the public realm. The design and treatment of these desire lines will enhance their use and choices to walk and cycle over car use.

Parking

Up to 30 parking spaces could be provided on Blundells Road around the urban open space as short stay and drop-off/pick-up provision. Designed carefully, the inclusion of cars and vehicles in this space can add to sense of activity at the centre. Parking spaces should not obstruct pedestrian connectivity across the urban space or highway and should be grouped in no more than 6 echelon spaces or 3 parallel spaces (see also section d3 -Parking). They should not clutter the urban space allowing it to be used for formal / informal spill out space from the shops, offices or community uses. Parking spaces should only be provided as part of the highway treatment to Blundells Road (not requiring additional road access or circulation). Tree planting adds value helping to soften the street-scene through visual interest but can also be used as a tool for spatial separation and/or containment. Street trees will be used to separate groups of parking.

1. A shared surface roundabout junction in Poynton
2. Welwyn Garden City -Boulevard demonstrating a mix of users in the street
3. Shared surface including parallel parking and bus stops
4. Small areas of changes in paving can alter the priorities which are perceived in a street



Quantum

The layout shown overleaf (Fig. 3.18) provides guidance on how the key design principles to achieve a good neighbourhood centre and the requirement for a given quantum of uses (as specified within the Local Plan Part 2 Urban Extension policy) may be addressed:

- 1.9 hectares is designated to Primary School use representative of a 2 form entry school of 420 pupils. The layout demonstrates how the necessary areas of outdoor space (hard playground and green playing field) could be accommodated in the context of the overarching design principles for the Neighbourhood Centre. A temporary access onto Blundell's Road for the primary school must be ensured in the first instance to enable early delivery of the school.
- A 2 hectare area is indicated for a variety of uses including shops and community buildings. The layout indicates the use of mixed blocks and buildings. This is in order to diversify the Neighbourhood Centre and co-locate as many complimentary uses as possible. The residents of the Urban Extension, should be encouraged to utilise, as far as possible, local facilities and reduce the number of car journeys. The vitality and density at the Neighbourhood Centre can also be supported by a quantum of residential accommodation at the Neighbourhood Centre, most likely included over ground floor uses.

Establishing the exact mix and viability of uses which come forward to create the Neighbourhood Centre is not within the

scope of this Design Guide but the spatial arrangement of buildings and spaces as shown overleaf (Fig. 3.18) provides the flexibility for a range of uses to contribute to the design principles of a distinctive, attractive and legible neighbourhood centre.

Frontage

Development at the Neighbourhood Centre should maintain a perimeter block arrangement in order to ensure the buildings front onto the surrounding streets and spaces. It is likely, due to the uses proposed at the Neighbourhood Centre, that service areas and car parking is required. These functional elements should be contained at the rear of the block preserving the front of the building as a sociable and attractive frontage to the public realm.

Building Heights

The Neighbourhood Centre should be recognisable as a destination within the Urban Extension. It is appropriate therefore that development in this location is taller. A range of building heights should be considered in order to create landmarks at key locations and in order to provide the mix of uses necessary to create a vibrant urban centre (eg. offices and residential uses on 2nd and 3rd storeys above retail at ground floor).

There should be no single storey buildings on the frontage with Blundells Road. In the case of the primary school building, every effort should be made (through design and legal negotiation) to add additional floors to the building by combining uses. Where this is not possible, the architecture of the primary school

building should address the built scale of surrounding streets. This may include generous internal volumes equivalent to a double height space or 2 storey development where it fronts the Urban Open Space.

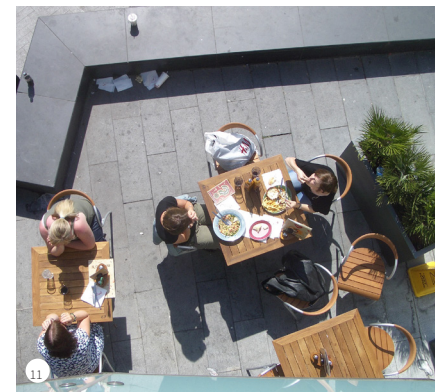
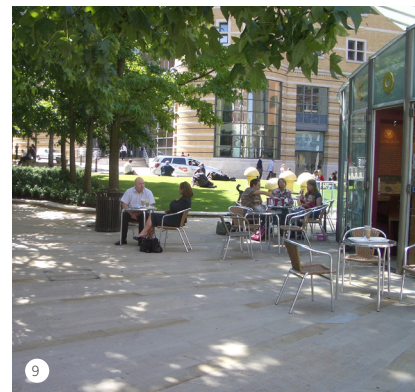
Shared Community/School Facilities

The provision of the Primary School requires an accompanying amount of outdoor space, hard playground and green playing field. Playing fields and sports pitch facilities would benefit the community as well as the school. The sharing of these facilities, achieved by the careful management of access and security in and out of school hours would make best use of the space allocated to green open space at the Neighbourhood Centre. Any public use of school facilities would need to be agreed with the school provider, once they are known.

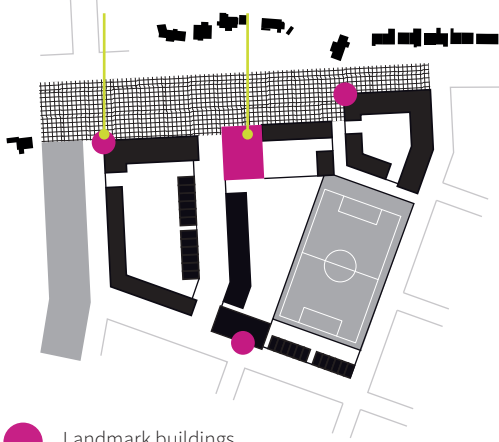
Surrounding Residential Development

The Neighbourhood Centre will include residential use as part of creating a mixed centre. Beyond the centre, the immediately surrounding residential development should reflect the scale and architecture of the centre. Development at the Neighbourhood Centre will form perimeter blocks which create frontage to the surrounding streets. Likewise surrounding development should face the Neighbourhood Centre.

5. Welwyn Garden City: Buildings landmark the corner of the block, retail with residential above and overlooking a green boulevard
6. Magdalen Road, Exeter: popular local shopping with short term parking
7. Birmingham: A busy, flexible open space
8. Generous public realm, including planting gives a good setting for shop fronts
9. Informal cafe spill out helps the atmosphere of a small square
10. A shared street where pedestrians have priority
11. Small cafe areas outside in the public realm



Taller buildings located at the corners of the Neighbourhood Centre help identify it from a distance



● Landmark buildings

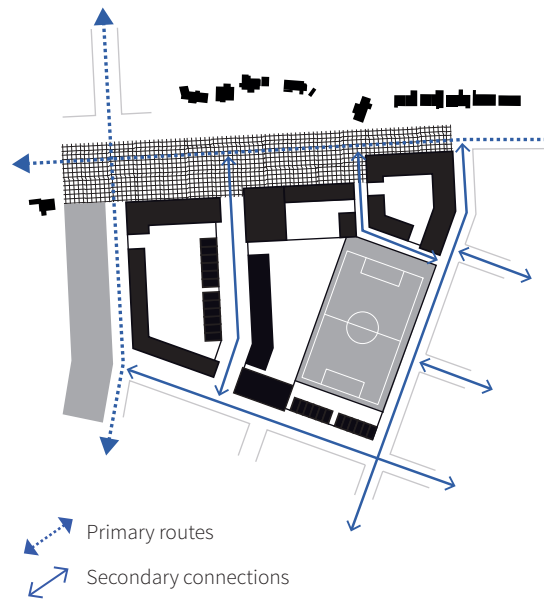
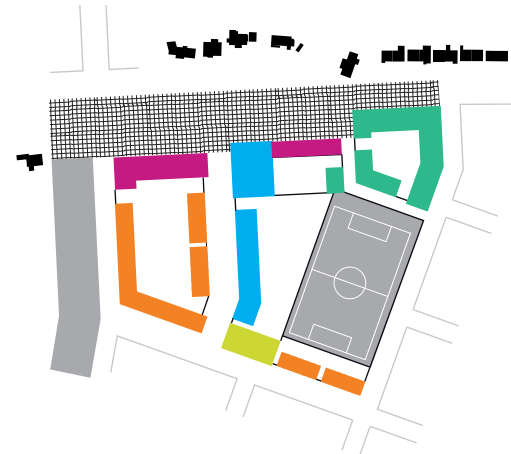
Landmarking

Carefully positioning key buildings and designing elevations to landmark the Neighbourhood Centre.

Key Buildings

Important Neighbourhood Centre buildings are located considerately with primary and secondary frontages.

- Community Facilities
- School
- Mixed use retail/commercial/residential
- Housing
- Creche / Nursery school



Routes

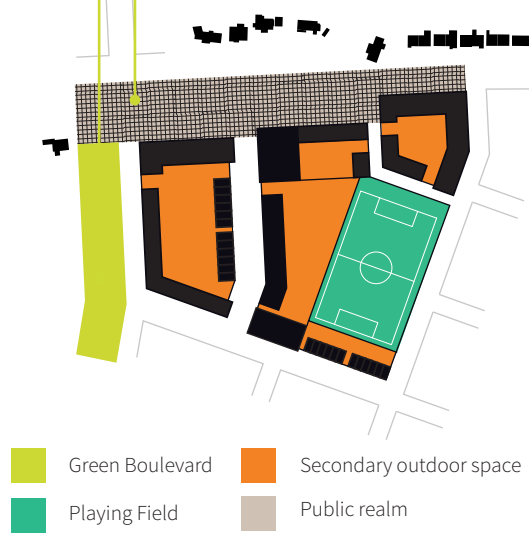
The layout of buildings in the Neighbourhood Centre emphasise the route through the site, with all secondary connections leading to principle routes.

Public/Private

There is a hierarchy of public to private outdoor space, with the central plaza and green boulevards prioritised.

Secondary outdoor space is visibly sheltered by surrounding buildings and clearly more private.

Green Boulevard
An Urban Open Space which gives priority to people and activity rather than the management of the vehicle.



- Green Boulevard
- Secondary outdoor space
- Playing Field
- Public realm

The new junction to Blundells Road from the link to the new A361 junction is an important place of arrival. The junction can create a distinctive and high quality urban plaza as well as manage traffic by employing a shared space philosophy.

The School main entrance from the Urban Open Space

Tree lined boulevard

Buildings to the east of the Neighbourhood Centre, fronting Blundells Road and more proximate to existing houses, up to 2.5 storeys in height.

Buildings which front the Urban Open Space are setback from Blundells Road to reflect the building line of existing buildings

Larger areas for car parking associated with shops, offices or community uses are enclosed within blocks to avoid blighting the public realm

The Primary School (420 pupils) is designed to create a strong edge to the surrounding streets and encloses a generous provision of outside space

Sports pitch provision may be made available to the community, subject to negotiations

A creche or pre-school facility located near to school to benefit from the shared use of outdoor play and green space

Development that surrounds the Neighbourhood Centre should be highly permeable to allow direct access to the facilities there

A secure boundary to the perimeter of the school

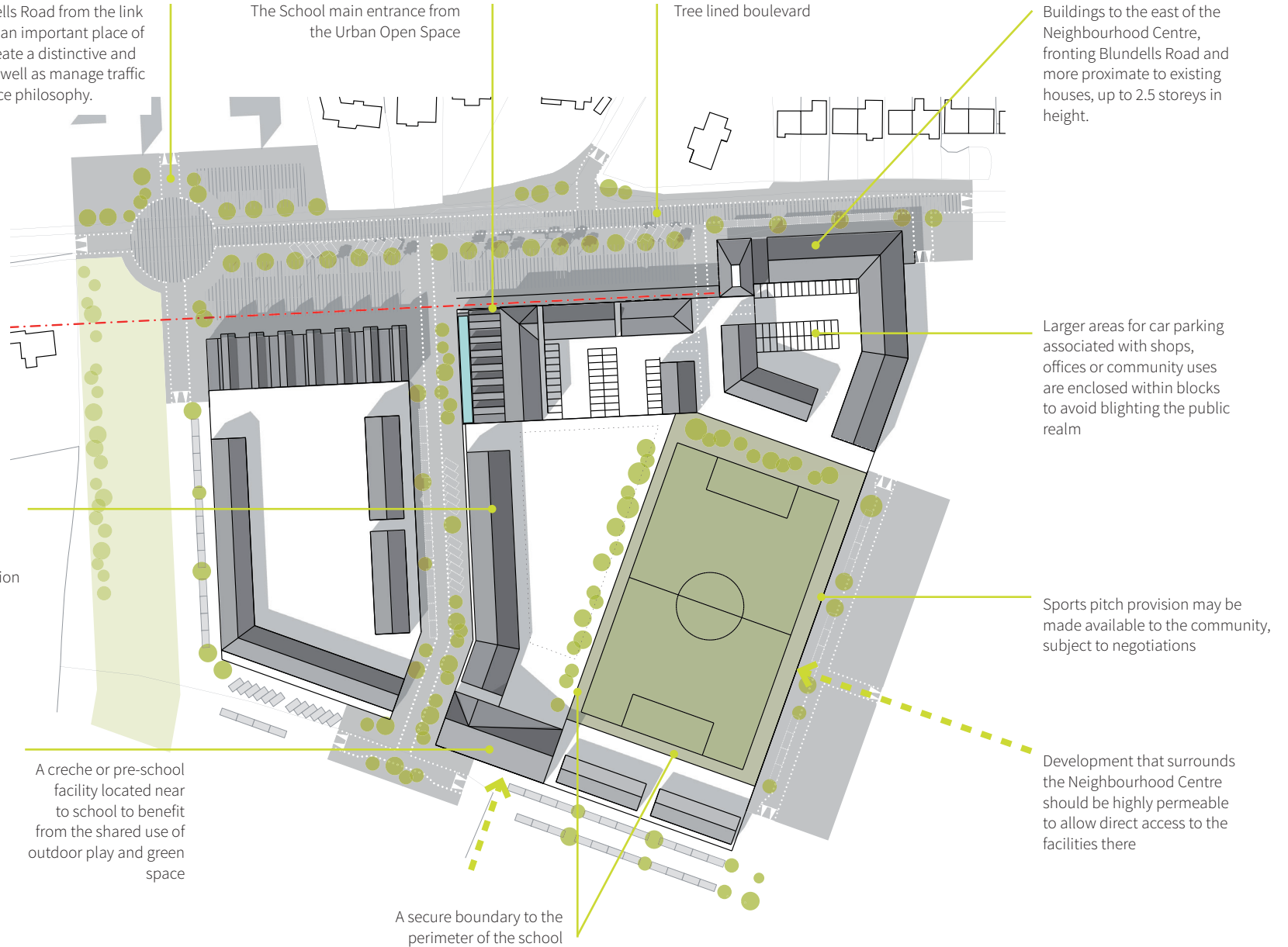


Figure 3.18 Neighbourhood Centre Indicative Layout

b2



Figure 3.20 Key Area Plan

The NHS Site

The NHS site serves as a key component of the Urban Extension's principle of Centre to Edge, providing a gateway to the North Eastern residential character area.

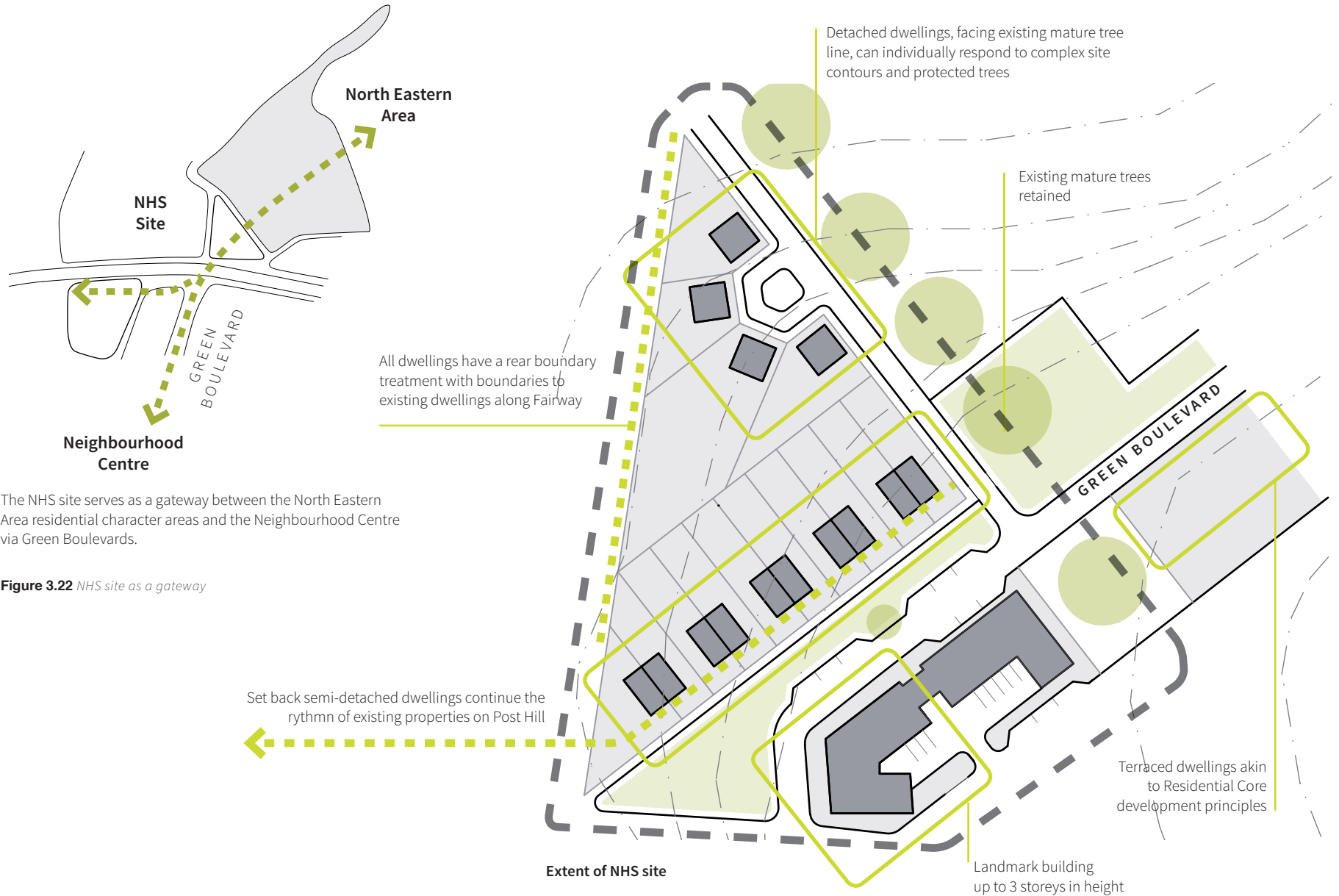
The site should act as a landmark to Blundells Road that compliments the Neighbourhood Centre in scale and density. This, in association with improved public realm infrastructure and traffic calming measures to Blundells Road, endeavours to bridge the connection between the NHS site, North Eastern Area and the Neighbourhood Centre.

To this end, the entrance carriageway is to be designed as a green boulevard, the highest hierarchical new street typology.

The building typologies on the north side of the proposed green boulevard reflect the grandeur of a green boulevard whilst respecting the existing neighbouring Post Hill properties.

The existing mature trees are a valuable asset to the site that are to be retained. The rear of the NHS site has a relatively steep terrain. In response to this, detached dwellings can respond individually to the topography.

This guidance allows for the site to include the provision of elderly care or a new care home as a building of grander scale. With a building of adequate landmarking, however, this corner plot may also take the form of a multi-storey residential property.



The NHS site serves as a gateway between the North Eastern Area residential character areas and the Neighbourhood Centre via Green Boulevards.

Figure 3.22 NHS site as a gateway

Figure 3.21 NHS Site block plan

b3

Employment

The Masterplan SPD for the Urban Extension seeks to create an integrated development. It therefore represents a mix of uses in order to establish a sustainable new environment.

Throughout the Masterplan SPD the mix of uses includes approximately 6 hectares of employment use principally B1a & B1c.

Some compatible employment space is proposed to be included at the Neighbourhood Centre with the remaining majority of employment development identified to the north west corner of the Masterplan SPD.

While it is acknowledged that the location of the employment provides easy access to the A361 via the new proposed junction to the north of the Urban Extension, the employment area should also be designed to integrate with the neighbouring Blundell's Road Conservation Area and school playing fields as well as the proposed residential areas of the Urban Extension. It presents a real opportunity for new residents of the Urban Extension to live and work within the area.

This Design Guide outlines how the employment area can:

- Be of a design, scale and mass across the employment site such that it provides for a mix of occupiers and

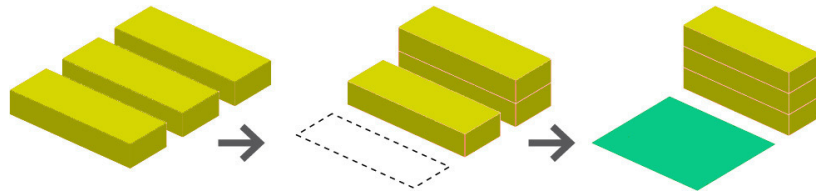


Figure 3.23 Key Area Plan

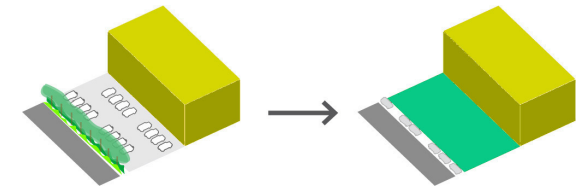
size of business uses which harmonises with the surrounding residential areas (building heights, primary accesses, building relationship to the streets and parking arrangement).

- Be organised around well designed streets which are sympathetic to pedestrians and cyclists and create pleasant environments within which to spend time as well as serving a circulation and access purpose.
- Be flexible, allowing for the possibility that over time occupation of the employment space may vary.

The employment area is to be considered with reference to the



The development of higher density, multi-storey commercial use allows for the introduction of important green infrastructure.



Parking is to be accommodated as either on-street parking or allocated private parking courtyards to the rear, presenting the frontage and streetscape as designed public realm with green infrastructure.

Figure 3.24 *Creating a quality public realm in employment areas*

same framework established in the Masterplan SPD as well as contributing strategically to the sense of Centre and Edge, enhancing the legibility of the place and promoting sustainable movement around and beyond the Urban Extension.

In this way the employment area will become an integrated part of the overall place. It is important that the employment area does not compete with the Neighbourhood Centre as a place that people during the working day use to meet, spend time and eat or drink. This can be avoided by:

- Avoiding providing any cafes or food outlets in the employment area

- Structuring the streets to enhance the legibility and signposting towards the Neighbourhood Centre. It is a short walk away and where streets and open space are provided suitable to the hierarchy, working people can be encouraged to use the neighbourhood centre as a destination during lunch breaks and before or after work activities.

Overall, the employment area can be an inviting, attractive and sustainable place to work and integrate well with the whole Urban Extension if it is designed with the following design objectives in mind:

- Purposeful landscape

- Promoting a strong, legible relationship with the neighbourhood centre to provide all day activity at the heart of the Urban Extension
- Be accessible, permeable and, via a series of connected streets, promote sustainable movement to and from the area
- Provide a series of plots which contribute to the quality of the environment but which are also flexible, not just to changing employment occupiers, but also to future changes in use.

Purposeful landscape and public realm are sorely lacking within

1. Residential

- Storey heights to have regard to the close relationship with existing properties.
- Semi-detached dwellings facing Blundells Road

2. Mixed Use

- Commercial use at ground floor, residential use above
- Built form and scale sensitive to a residential street

3. Employment

- Higher profile commercial use
- Focal building on arrival from A361

4. Employment

- B1a and B1b use typically
- Plots sensitively backing on to mixed use typology

5. Employment

- Commercial and potentially storage/distribution use (B8)
- Built form to reflect use, allowance for flat or shallow pitched roofs and multi-storey buildings
- Allocated private parking to the rear

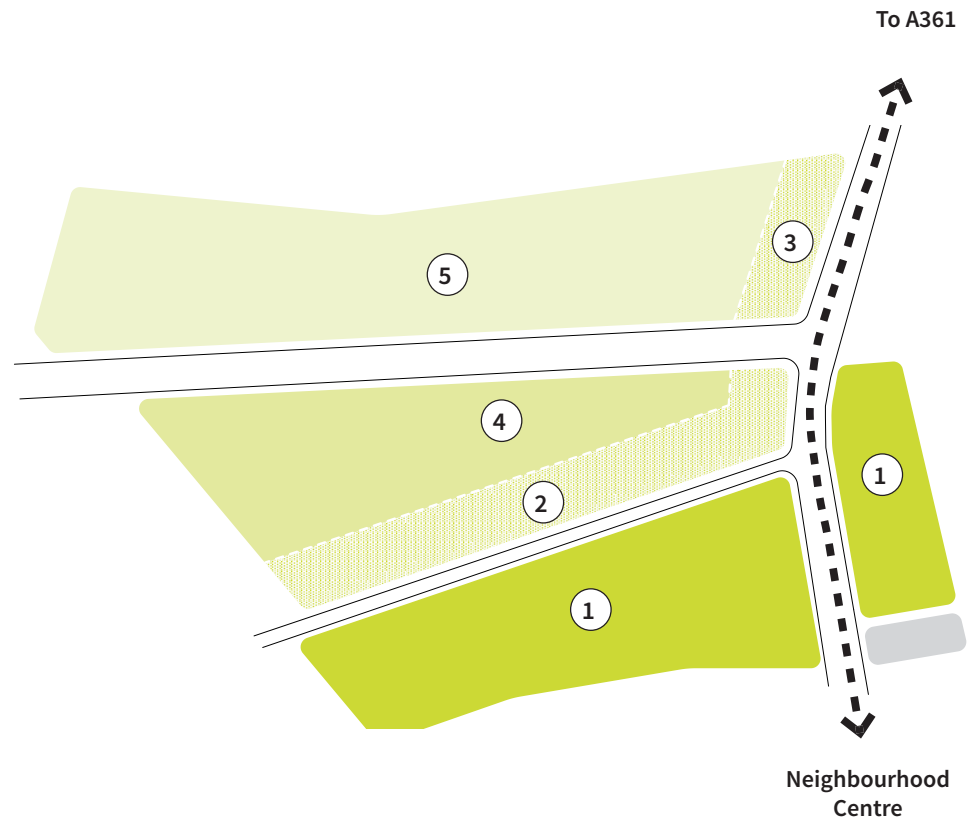


Figure 3.25 Employment Area Diagram



existing 'business park' environments. Often these environments are seen on the outskirts of towns and cities and promote car accessibility and circulation above the enjoyment of the environment by pedestrians. The result is an unsatisfactory environment and pattern of behaviour where people spend lunch breaks or time before or after work in the car travelling to a nearby supermarket or shop.

The aspiration to create an integrated, attractive employment area seeks to reverse this trend and provide a work environment which is enjoyable to be in and which is within easy walking distance of a range of facilities and enjoyable public spaces.

Rather than just screening unsightly car parks or service areas, the landscape should serve a purpose in enhancing the legibility of streets, providing attractive spaces for people to spend time in and contributing to the sense of Centre and Edge which underpins the formation of this new Urban Extension.

Built form should be of a suitable scale and provide main entrances onto the street. Buildings should be at least 2 storeys high, in order to create streets which have good enclosure and are not dissimilar to the human scale of the surrounding residential streets. Parking and servicing should be behind buildings or within blocks, with access appropriate to the type of vehicles associated with the use, but to enable the street at the front of the building to be truly public realm.

The street sections shown articulate the public realm and the built scale variations through the Employment Area. In addition to these, the development edge treatments to the North and West are resolved further in the respective threshold sections of this Structure chapter.

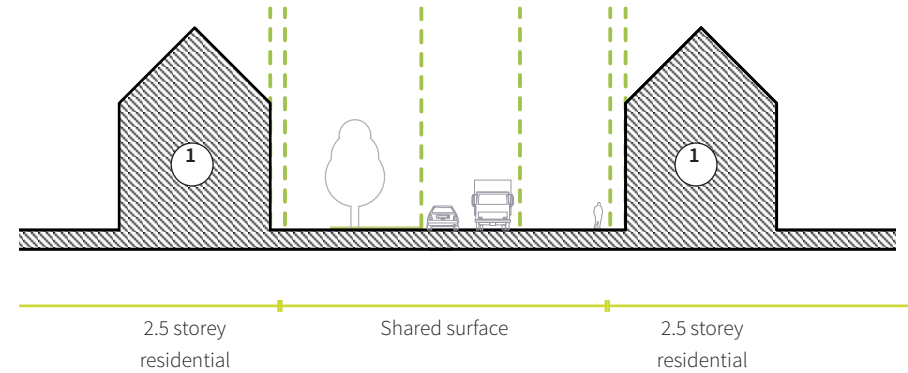


Figure 3.26 *Employment Area Layout*



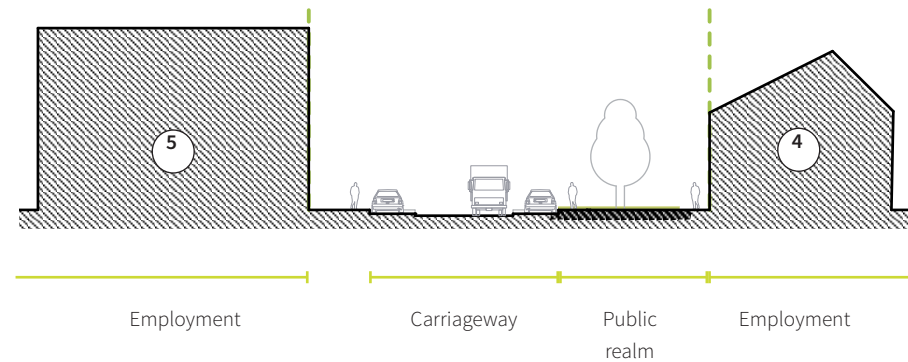
Section A-A

- Principal carriageway connecting the Neighbourhood Centre with the new junction to the A361.
- To be treated as an extension of the urban design of Blundells Road.
- Dwellings to be 2.5 storeys high, following principles of Green Boulevards.
- Narrow frontage threshold to emphasise the urban, higher density environment.



Section B-B

- Continuous green infrastructure public realm to the south side of the street.
- Private rear parking allocations in addition to on street, parallel parking.
- Industrial building scale only suitable backing on to A361.
- Building form and scale increasingly sensitive to human scale as buildings near residential development and Blundells Road.
- Refer to Northern Edge treatment section for natural screening of development to A361.



Section C-C

- Where employment use faces a residential dwelling, scale and form is sensitive to a residential street, allowing for future flexibility.
- Mixed use approach may be used with employment at ground floor and residential above.

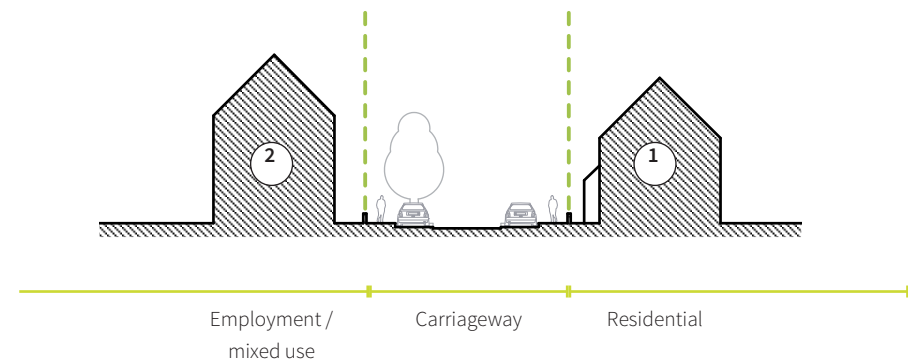


Figure 3.27 Employment Zone Sections

b4

The Blundells Road Corridor

As an existing route between the Post Hill area and Tiverton Town Centre the Blundells Road Corridor fulfills a very important role in the establishment of a new sustainable community at the Urban Extension. However, in its existing role it is not currently designed to suit the purpose of the new urban extension.

Along its length the Blundells Road Corridor has a variety of relationships with the Urban Extension. The design of the corridor should deal in different ways with each of these relationships, summarised by these 3 sections across the road corridor:

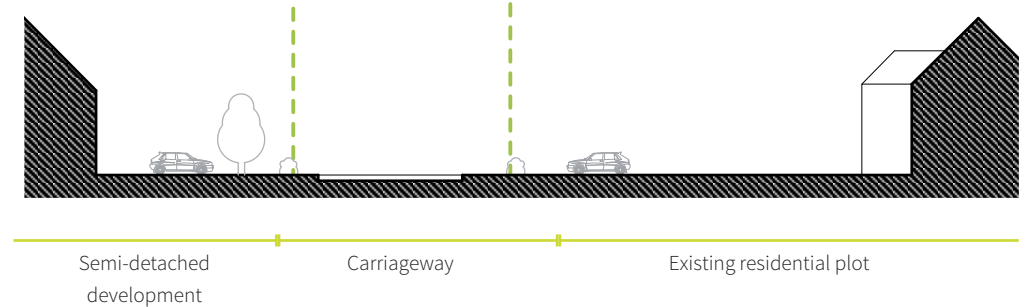
- Blundells Road properties and new north western area;
- Blundells Road properties and the Neighbourhood Centre;
- Blundells Road properties and new residential properties.



Figure 3.28 Blundells Road Corridor (Masterplan)

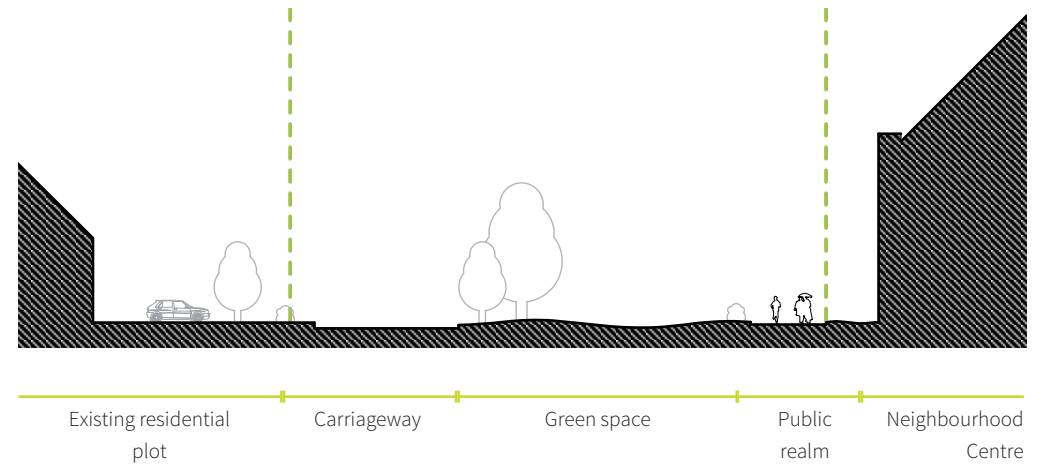
Section A-A

- New residential development to the north to reflect existing southerly development.
- Semi-detached typology (refer to Employment area layout; Chapter b3).
- Carriageway traffic calming, pavements and cycle infrastructure in line with MDDC design development.



Section B-B

- Formal civic public realm.
- Civic scale of built form to south side of road - refer to Neighbourhood Centre; Chapter b1.
- Carriageway narrows. Public transport infrastructure and some parking.
- Neighbourhood Centre maximum 2.5 storeys.



Section C-C

- High density Residential Core development facing Blundells Road.
- 2.5 storey buildings encouraged.

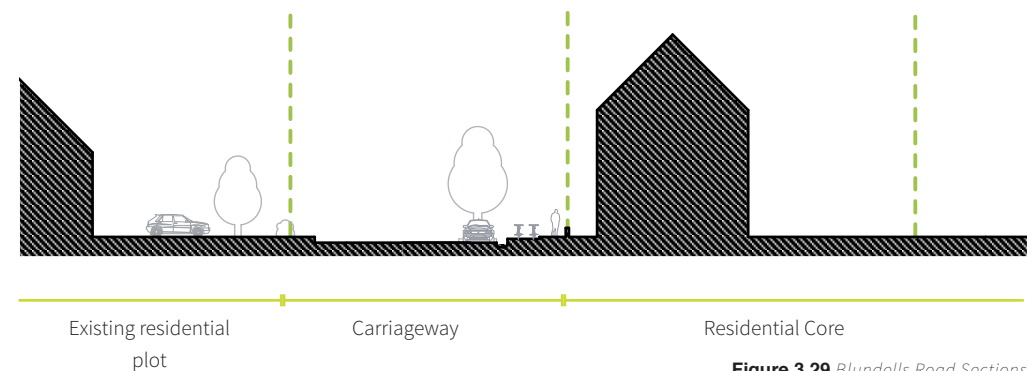


Figure 3.29 Blundells Road Sections

b5

The Western Edge Relationship

Key to a sensitively delivered development is an awareness of the existing and potential conditions of the development's boundaries.

The West Boundary will be highly visible from the easterly bound carriageway of Blundells Road and has an important relationship with Blundells School and the setting of the Conservation Area. Visually this is important as is a consideration for the school potentially developing areas of its site ownership in the future.

Important in the delivery of the West Edge is to ensure sensitivity and ensure a sense of identity, re-inforcing the Centre to Edge principles.

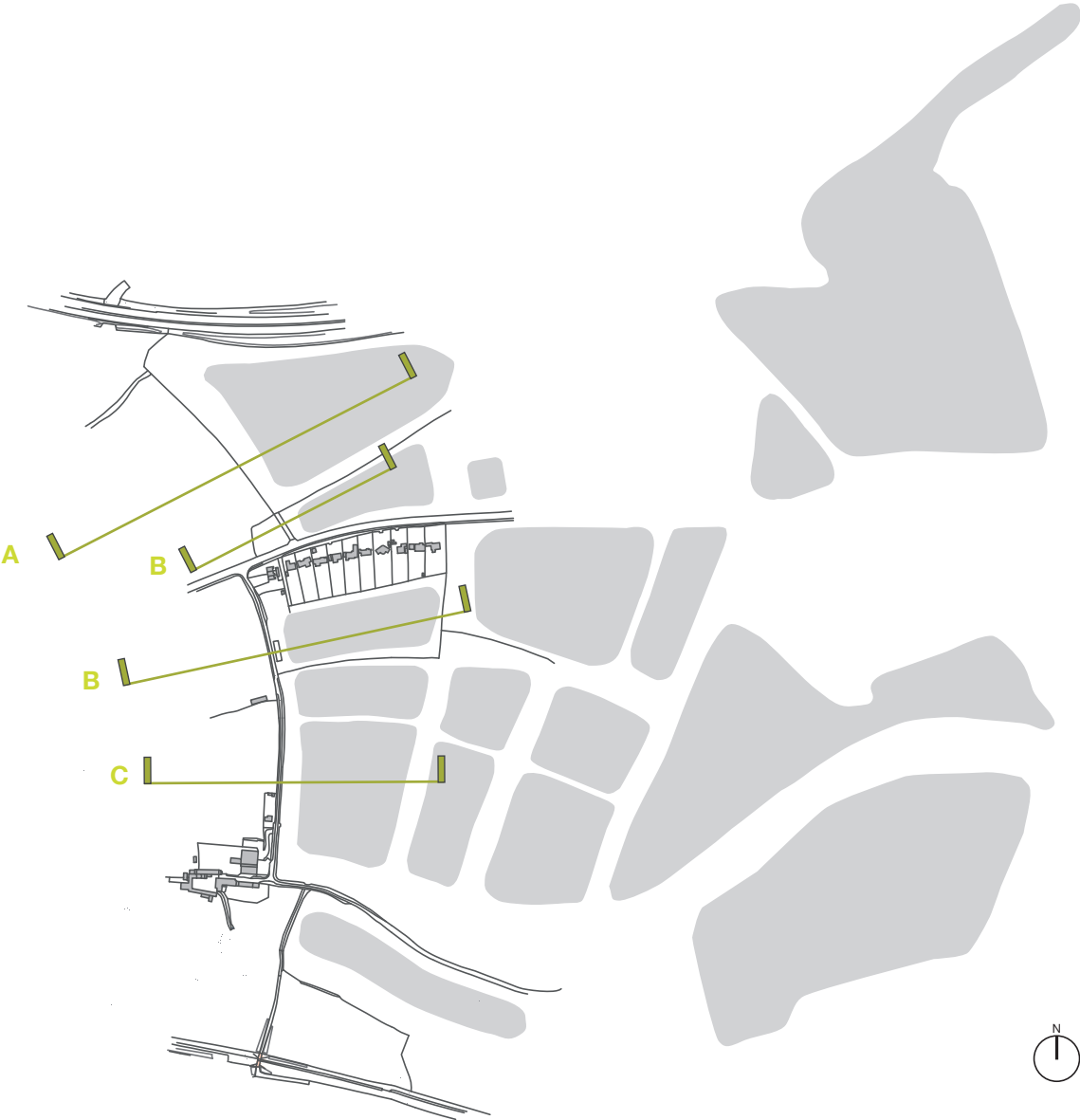


Figure 3.30 Western Edge Masterplan

A: Employment

- Buildings to have frontages to road and therefore the Blundells school fields
- Existing mature planting to be maintained and enhanced.
- 2+ storey units to also be narrower (width) than typical employment use to achieve a permeability of built form (Refer to B3: Employment chapter).

B: Residential Core

- West end of terraces to be a wider dwelling with frontage on to West Manley Lane.
- Wider dwellings/plots. Hipped roofs to help soften terrace edge.
- Planted boundary treatment to road.

C: Parkland

- Detached dwellings fronting onto West Manley Lane.
- Planted boundary treatment to road.

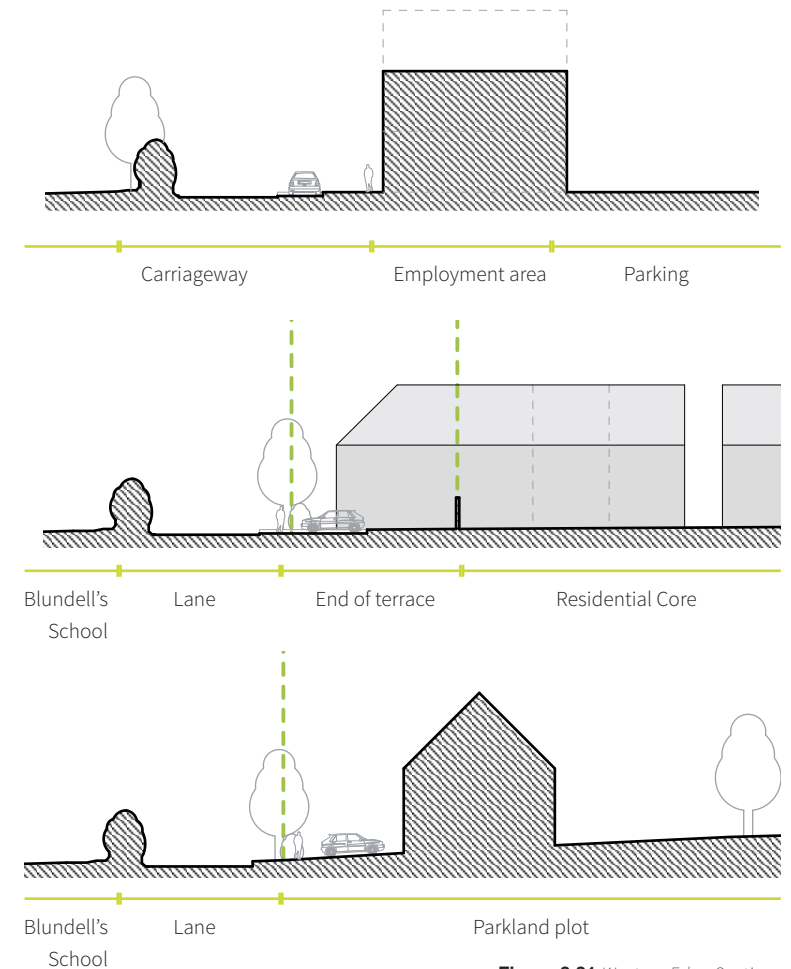


Figure 3.31 Western Edge Sections

b6

The Northern Edge Relationship

The Northern Edge is principally defined by the relationship of proposed development with the A361.

Residential and employment developments (the proposed uses as allocated in the Urban Extension Masterplan SPD) must therefore respond sensitively and uniquely to this context.

In honouring the principles of the Masterplan SPD, the employment area's relationship with the A361 is accepted to be relatively unique whilst the North Eastern residential area's Northern Edge relationship adheres to the development principle of Centre to Edge.

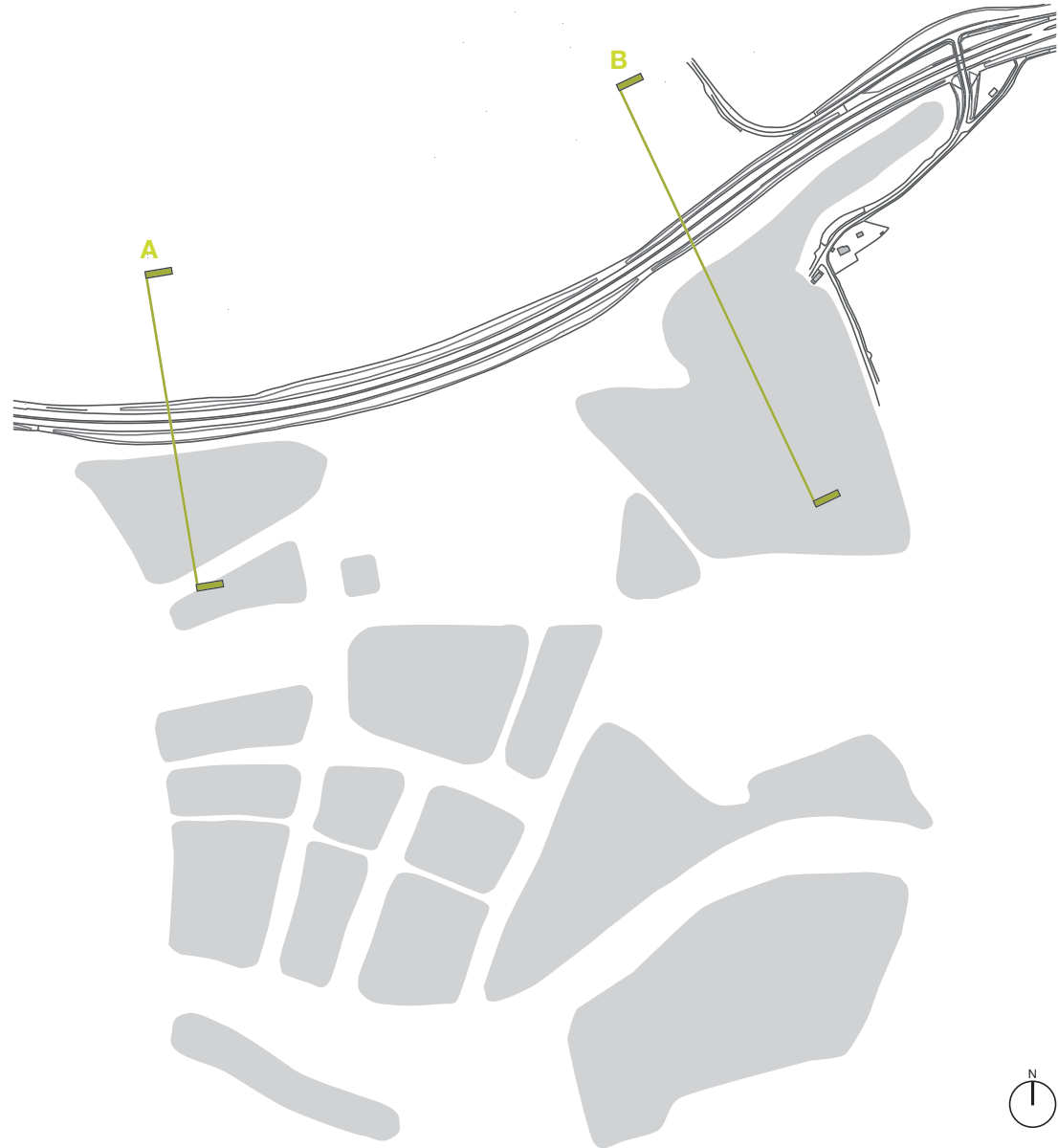
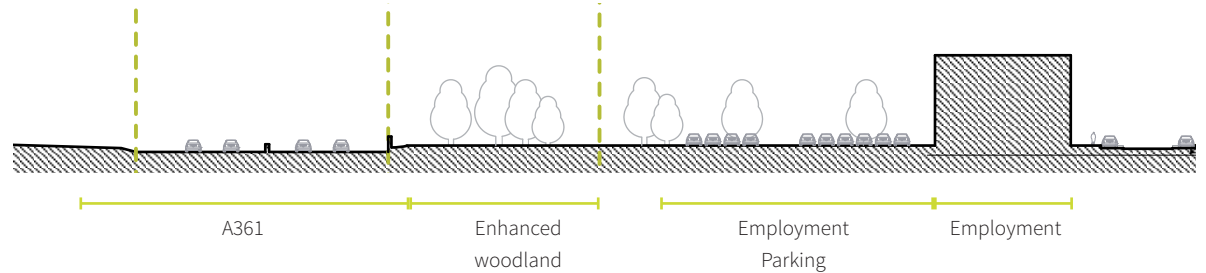


Figure 3.32 Northern Edge

A: Employment

The existing and enhanced woodland to the north of the employment zone serves as a buffer between the A361 and the built development.

Parking for this development should be set behind the buildings, further serving as a buffer and enabling the street scene forward of employment buildings to be prioritised for the pedestrian.



B: North Eastern Area

The north boundary of the North Eastern residential development will also be visually screened by enhancements to existing mature planting. Development plots are set back approx. 75m from the A361 and development should include:

- Dwellings separated from the A361 by the new access road
- Properties closest to A361: Side elevations (minimal fenestration) to face A361

(Refer to North Eastern Area character chapter: C2)

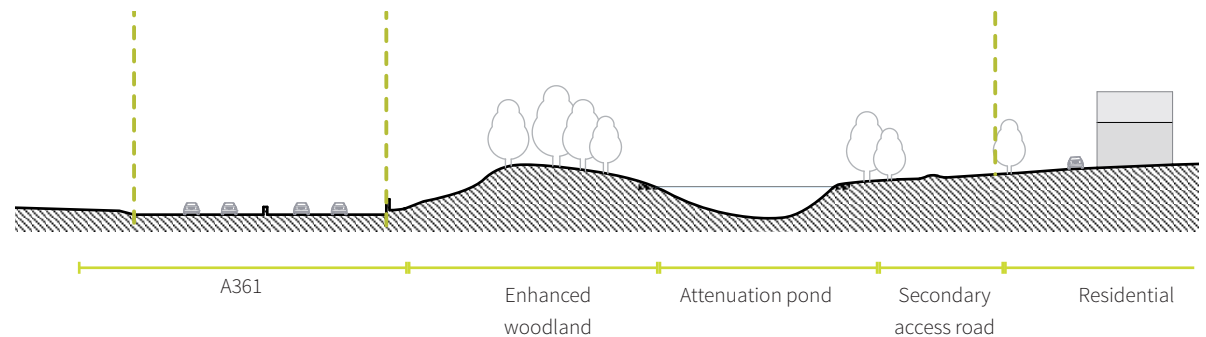


Figure 3.33 Northern Edge Sections

Streets: Centre to Edge

The hierarchy of streets is derived from the structure of the Masterplan SPD and relates to the relationship between Centre and Edge. The variety of streets throughout the Urban Extension should be contrasting but at the same time create a clear structure which is legible and aids navigation and sense of place.

The guidance on street design overleaf outlines how variations in street width and treatment can create a clear distinction between streets in the hierarchy while maintaining connectivity and legibility.

The character and role of each street can be defined by variations in the following:

- The type and scale of buildings
- Widths of elements in the street section: carriageway, pavement, traffic calming, and threshold dimensions
- Arrangement of parking and tree planting

In general the streets nearer the Centre should be formed using taller buildings, wider pavements and carriageways but narrower or no setbacks of the buildings from the street. Nearer to the edge building to building widths are more generous yet carriageways and pavements are narrower.

An exception to the street hierarchy are the Green Boulevards. The Green Boulevards extend from the Centre to the Edge and bring continuity and connectivity to the whole Urban Extension.

Formal tree planting within the street scene will be required. Main and secondary streets will be specifically designed to accommodate trees and their successful establishment. It is important that trees have sufficient space to grow to a reasonable size without adversely affecting infrastructure, property or residential amenity. Where space allows suitable statement trees should be planted to provide focal interest and greater canopy cover.

A long term desired effect should be agreed and any maintenance required to achieve this effect must be established, for example identifying areas where rotational pollarding would be suitable for any particular tree, if a more formal appearance is desired. Smaller, more compact tree species may also be selected where space is limited. Due to the rural setting of the Eastern Urban Extension, native species are more desirable.

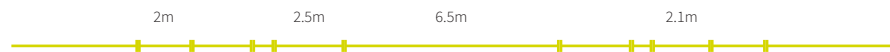
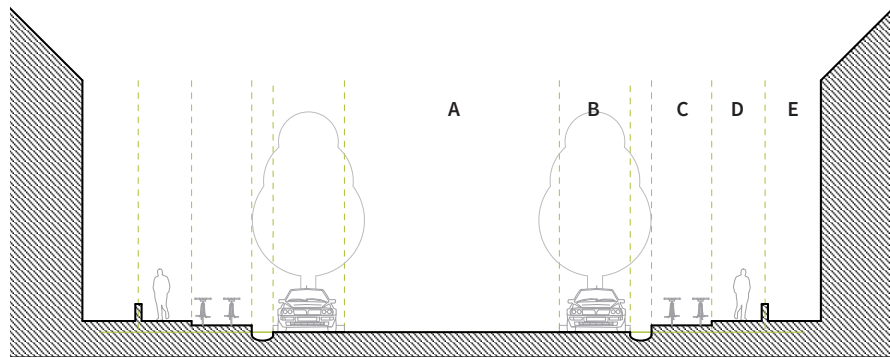
A detailed planting specification, using the most up to date technology, to include root barriers, root directors, tree pit connectivity, irrigation, paving styles and staking/guying methods will help to ensure a good future relationship between the tree and the built environment, as well as offering improved conditions for optimum tree health.

A detailed plan showing the planting location, species, size, and quantities, and planting densities of trees will be required.



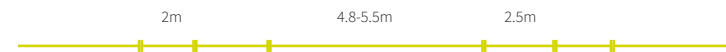
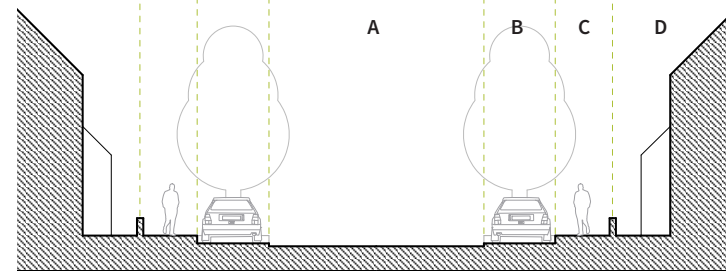
1. Informal cafe spill out helps the atmosphere of a small square
2. Generously proportioned pedestrian routes
3. Central residential streets maintain a domestic quality
4. Green infrastructure characterises streets to the edge

Primary Street/Boulevard



- A - Carriageway
- B - On Street Parking
- C - Cycle Lane
- D - Pavement
- E - Plot

Secondary street

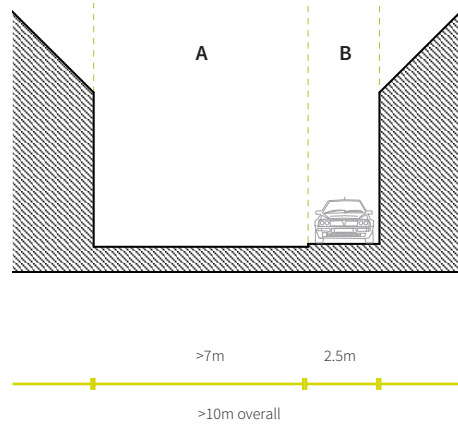


- A - Carriageway
- B - On Street Parking
- C - Pavement
- D - Plot

Centre

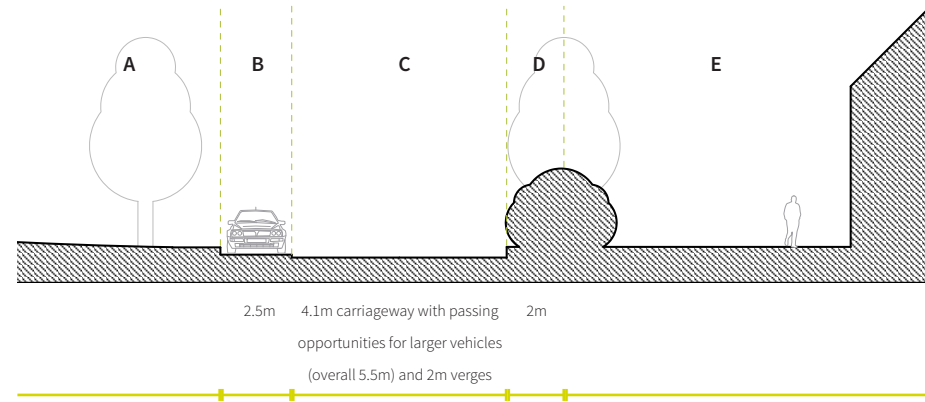


Mews street



- A - Carriageway (shared surface)
- B - On Street Parking (on either side)

Lane



- A - Park
- B - On Street Parking
- C - Carriageway
- D - Pavement
- E - Plot

Edge

NB. Highway design will be subject to discussions with Devon County Council Highway Authority

b7

Streets: Green Boulevard

The Green Boulevards extend from the Centre of the development to the edge. The Boulevards themselves will vary along their length as they transition from the mixed use centre to the landscape edge. Their function is varied from creating the structure to the development and directing people towards the key destinations in the Urban Extension, to providing storm water attenuation capacity and ecological corridors.

The design of the green corridors and their alignment within the overall development layout should be carried out in coordination with the design of the built form and other infrastructure to ensure legibility and connectivity are achieved.

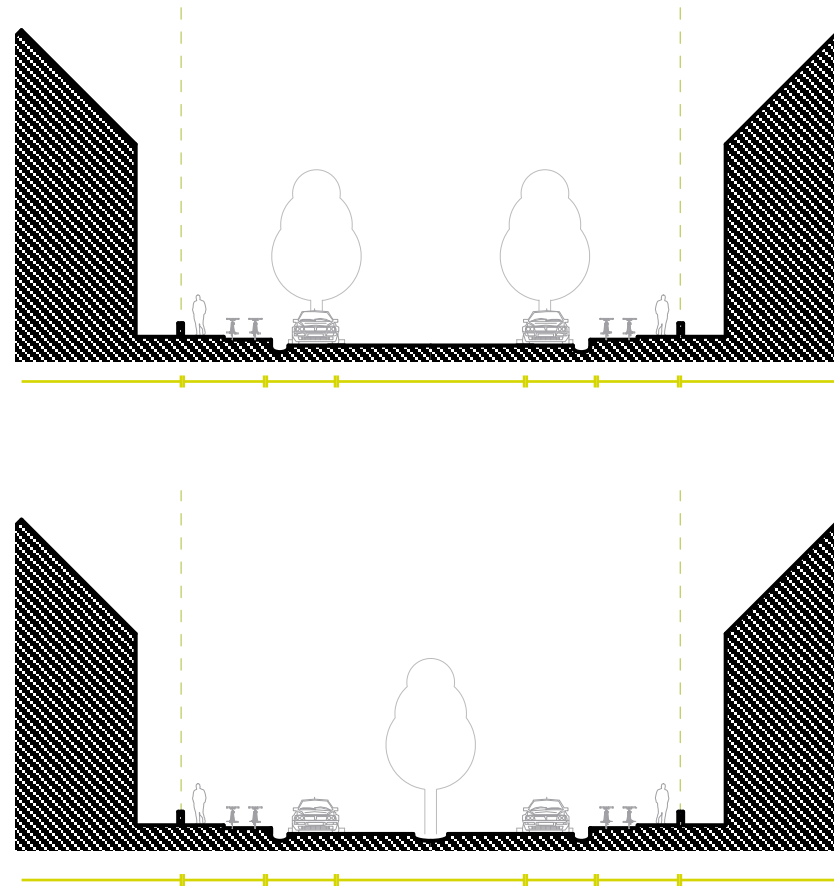
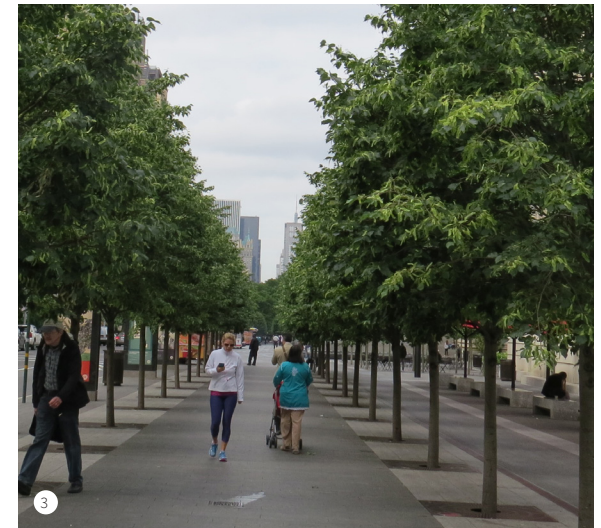


Figure 3.34 Green Boulevard Section Options

1. Play and other activities included within a wide street corridor
2. Integrating drainage and ecological asset within the green corridors
3. A formally arranged green corridor



b8

The Green Infrastructure Hierarchy

The variety of green space is to be articulated in line with the guiding principles of Centre to Edge. It is derived directly from the green framework of the Masterplan SPD.

The Design Guide outlines a range of spaces which vary according to location and relationships across the Urban Extension. Urban public realm defines the Neighbourhood Centre transitioning gradually to the natural landscapes at the periphery of the development via generous green boulevards as defined in Chapter b7: Streets. Experienced from edge inwards the landscape reaches in towards the centre to create a sense of place rooted in the surrounding landscape qualities and assets.

Pocket parks are intermediately distributed throughout the development.



Figure 3.35 Open Space Hierarchy Diagram

Open Space Hierachy

1. Urban Open Space	2. Residential Pocket Parks	3. Green Boulevard	4. Park Spine	5. Country Park	6. Other Green Infrastructure
<ul style="list-style-type: none"> • Urban in character. • Predominantly hard paved. • For civic and community purposes. • To include a portion of the Blundell's Road highway. • Some formal tree planting. • Includes some car parking (e.g. for surrounding shops) • To take reference from local centres eg. Bradninch. • Refer to Neighbourhood Centre structure diagrams (Chapter B1). • Drainage / SUDS can be incorporated. 	<ul style="list-style-type: none"> • Areas for play and outside space within residential areas for small scale recreation. • Green or hard in character to suit residential surroundings. • Up to 40m x 40m. • Minimum of 15m x 15m. • Typically located within 400m of residential area (all residents to be within 400m of a pocket park). • Drainage / SUDS can be incorporated. 	<ul style="list-style-type: none"> • Linear spaces with continuity from centre to edge. • Incorporate streets and routes which extend from the centre out to the edge (including vehicular traffic). • Linear planting, may include existing linear features such as hedgerows. • Create a legible structure to the layout of the whole settlement area. • Desirable pedestrian and cycle routes. • Drainage / SUDS can be incorporated. • Demand adequate frontage from buildings either side to ensure overlooking and to provide a sense of enclosure. • Minimum 25m wide, up to 40m wide. 	<ul style="list-style-type: none"> • South facing steep gradient. • East spine to serve as more formal, linear Green Infrastructure between residential character areas. • West spine permeating towards open countryside. • Opportunity for introduction of formal/informal play spaces (contributing to Neighbourhood Centre outdoor space provisions) where gradients allow. • Otherwise, largely unprogrammed space is welcomed. • Drainage / SUDS can be incorporated. 	<ul style="list-style-type: none"> • Mature existing habitats • Allow low density development at northern edge to permeate in to nature (West Manley Lane). • Pedestrian and cycle access through development to canal path and Sustrans route, connecting with Tiverton Centre. • Points of access to encourage movement. • Drainage / SUDS can be incorporated. 	<ul style="list-style-type: none"> • Existing green infrastructure is maintained (and emphasised) wherever possible. • Drainage / SUDS can be incorporated. • Provide shelter and privacy from road infrastructure. • Provide privacy to and from neighbouring land and dwellings.

Creating a New Residential Community

- c1 Residential Core
- c2 North Eastern Area
- c3 Parkland
- c4 West Manley Lane

- c1 Residential Core
- c2 North Eastern Area
- c3 Parkland
- c4 West Manley Lane
- 5 Existing Development

Creating a New Residential Community

This chapter defines each of the residential character areas. These have been established with direct reference to the Masterplan SPD. The areas are characterised by the guiding principle of centre to edge and fit within the green structure of the development, varying in form and density of development.

Central to the structure of each of these character areas is the relative position to the centre or the edge of the development.

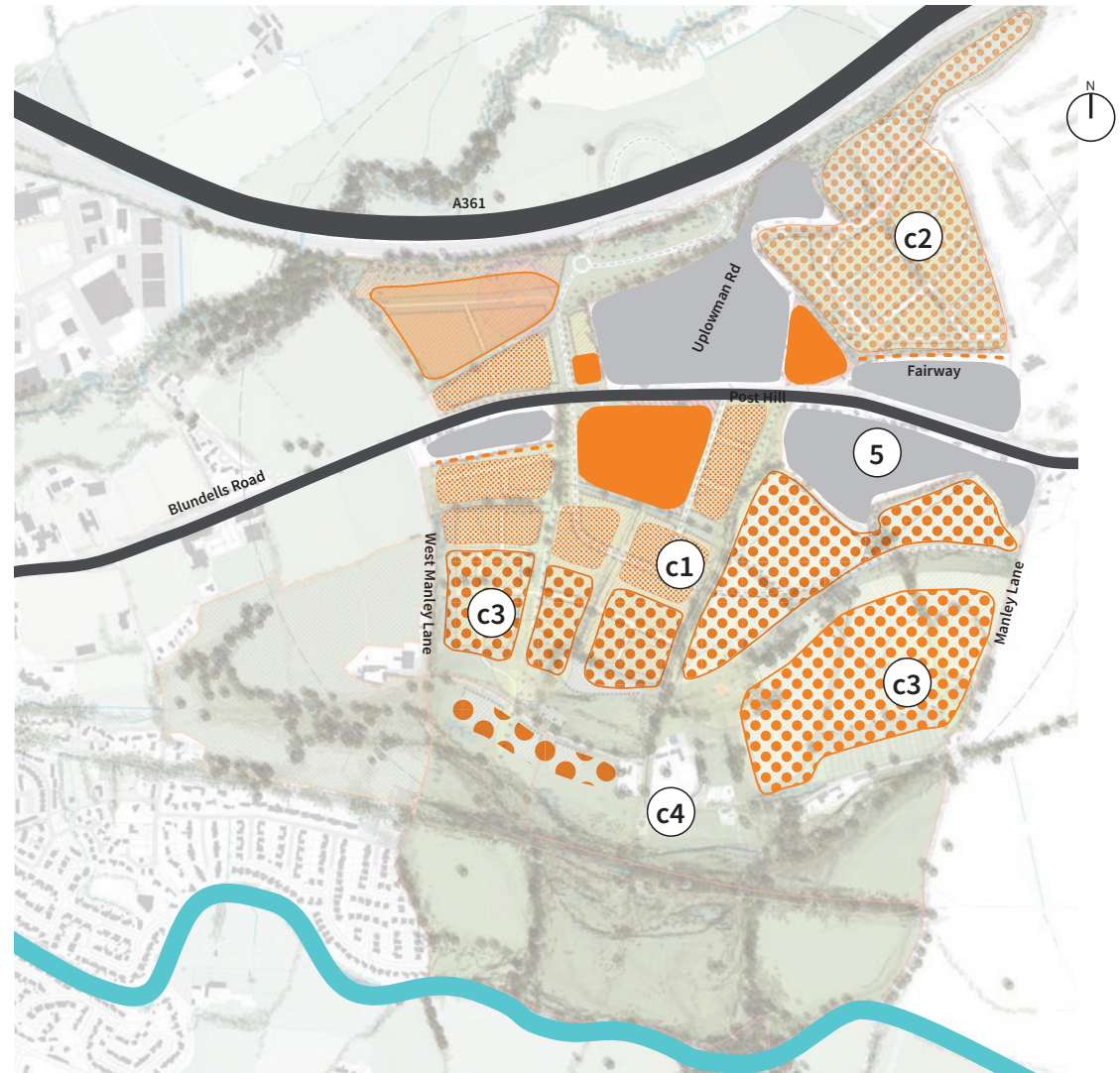


Figure 3.36 Character Areas

The Design Guide provides a series of example block plans for each character area. These example blocks plans, typify the approach taken in each character area but do not represent a specific block in the Masterplan SPD layout. Guidance should be taken from these example block plans to inform the site specific response in each character area.

c1

The Residential Core

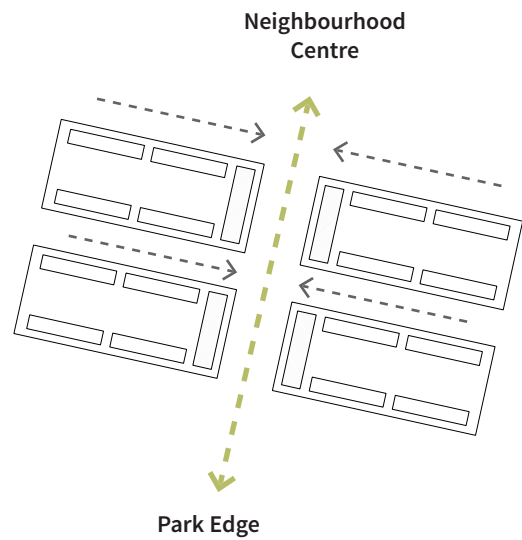
The Residential Core development area, as identified in the Key Plan, surrounds the Neighbourhood Centre of the EUE. It is the highest density residential development area and has the following characteristics:

- A direct relationship to Blundells Road.
- Adjacency to Neighbourhood Centre, providing, connections with local infrastructure and Tiverton centre.
- Clear but more distant visual connections with natural landscape.
- Distinctive yet achievable topography for development.
- West edge of development sharing an important boundary edge with Blundell's School.



Figure 3.37 Key Area Plan

Block Form	Plot	Typology	Height	Frontage
<ul style="list-style-type: none"> • 4 sided block. • No greater than 50m on its shortest side. • Block length no more than twice its width. • Block form aligned perpendicular to green boulevard. 	<ul style="list-style-type: none"> • Back to back plots only. • Smaller terrace ~ 5m x 20m. • Larger terrace ~ 6m x 25m. 	<ul style="list-style-type: none"> • Terraced houses in groups of 3-6 units. • Secure rear access to all dwellings. • Stepping of facades to between houses to be avoided. • Bay fronted dwellings encouraged. 	<ul style="list-style-type: none"> • 2 storey buildings on branch roads. • 3 storey buildings concentrated on block edge facing green boulevard. • Up to 2.5 storey buildings fronting existing residential development. 	<ul style="list-style-type: none"> • Up to 1m deep frontage for smaller terraces. • 2-3m frontage for larger terrace with low walled front boundary treatment.



The shortest edge of development blocks should face the Green Boulevard offering a direct link to the centre or the edge.

Figure 3.38 *Green Connections*

The adjacent table and these corresponding diagrams identify the parameters that should be followed.

The example block plans (for Residential Core and throughout this guidance) should be used as a guidance for developer proposals to follow the identified design parameters.

The rigid rectangular block form indicated is for ease of illustration and applications will need to respond to site specific needs and to the character area being developed.

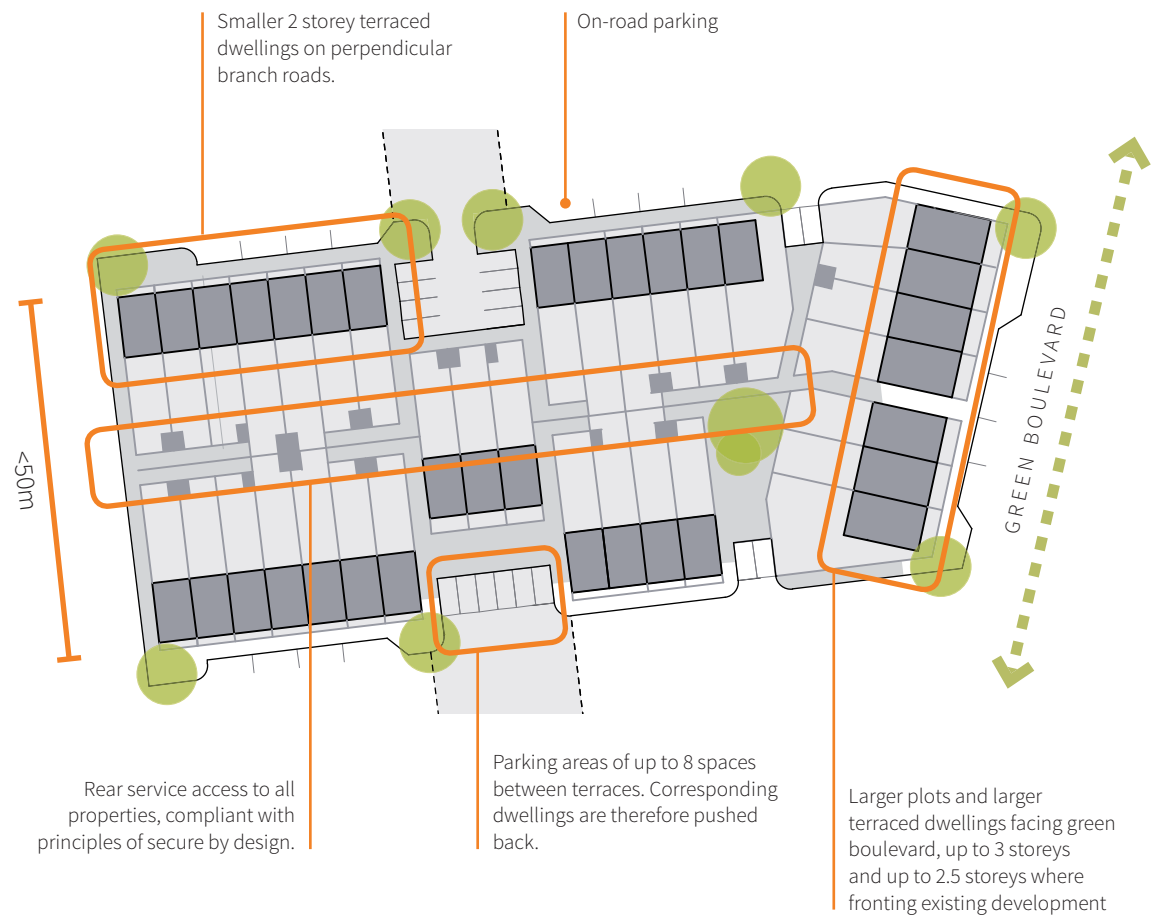
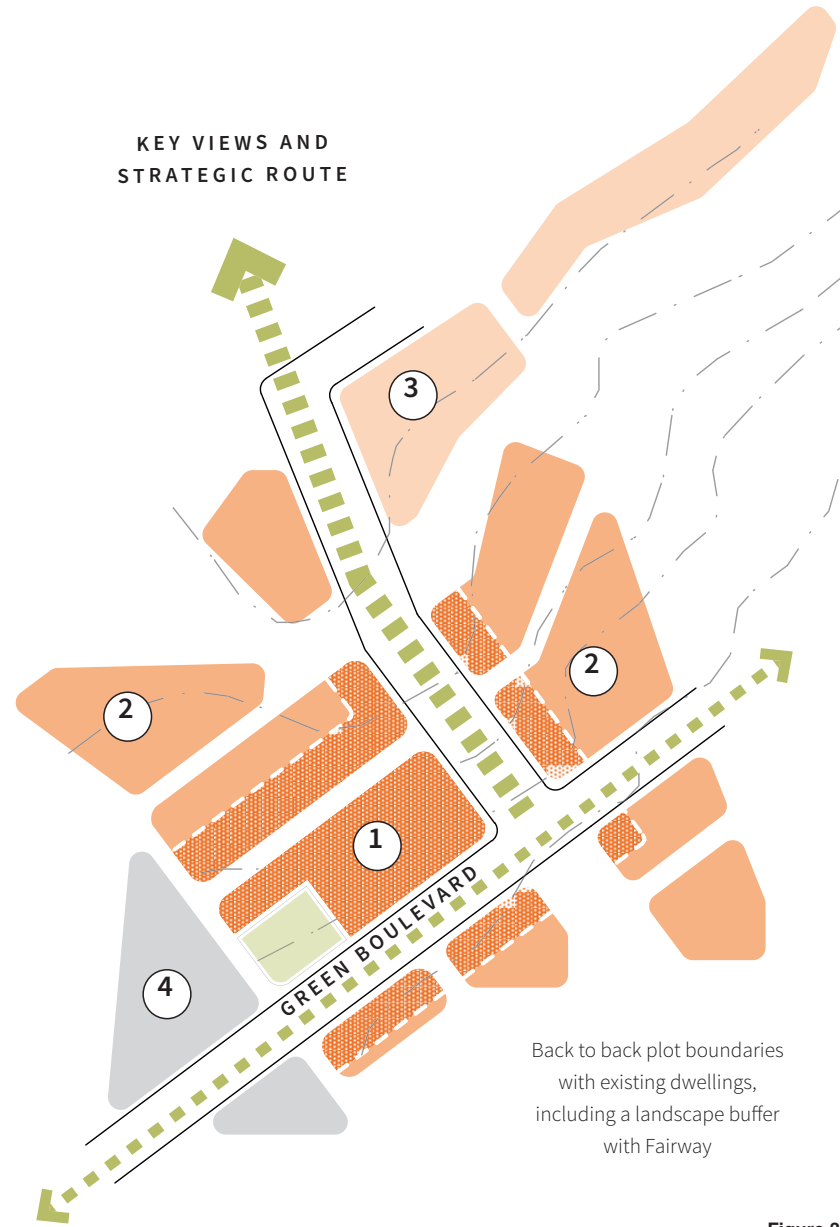


Figure 3.39 *Example Block Plan*
Orientation may vary according to site specific application

KEY VIEWS AND STRATEGIC ROUTE



- ① High Density
- ② Medium Density
- ③ Low Density
- ④ NHS Site

Back to back plot boundaries with existing dwellings, including a landscape buffer with Fairway



Figure 3.40 Key Area Plan

c2

North Eastern Area

Development at the North Eastern Area is one of the most important areas of development in the context of responding sensitively to both existing properties and the topographical characteristics of the site.

The North Eastern Area Key Area Plan (Fig. 3.40) depicts the overall structure that is proposed and the associated development densities.

The NHS site serves as the gateway in to the North Eastern Area which itself will be a higher density of development. A generous green boulevard articulates a grander sense of scale to the core of development that runs along the

ridge of the site.

Perpendicular to this is an equivalent key route that extends the strategic transport corridor to Uplowman Road. This serves as the main tributary of vehicular traffic across the site. Proposals should therefore provide for adequate pedestrian public realm and a drainage management strategy as identified in the Masterplan SPD. The north-south carriageway follows a gradient that is typically up to 1:12 but can have localised areas of 1:8 where there are no branching junctions.

Development across the North Eastern Area adheres to



Figure 3.41 Key Area Plan

the principles of Centre and Edge with density decreasing from the NHS site toward the north east boundary. Medium density proposed development backs onto existing dwellings, always with a back to back plot treatment.

c2

North Eastern Area: High Density

The high density residential development of the North Eastern Area has the following characteristics:

- Wide green boulevards connecting the North Eastern Area with the greater EUE development via the NHS site and a gateway open space.
- Extensive views to the North highlighted with perpendicular green boulevard.
- Terraced dwellings, with larger properties up to 3 storeys facing green boulevards, creating a sense of centre for the North Eastern character area.
- Development blocks and building typologies responding to the topography of the site.
- Parking in small bays between terraces and on street.



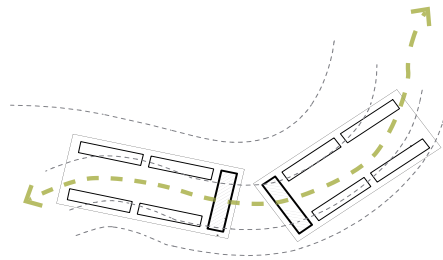
Figure 3.42 Key Area Plan



Figure 3.43 North Eastern Area Density Plan

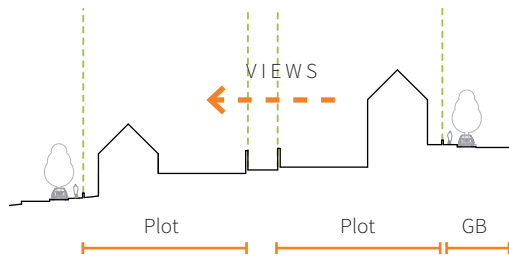
Block Form	Plot	Typology	Height	Frontage
<ul style="list-style-type: none"> • 4 sided block. • No greater than 50m on its shortest side. • Block length no more than twice its width. • Main route aligned parallel to contours. 	<ul style="list-style-type: none"> • Back to back plots only. • Smaller terrace ~ 5m x 20m. • Larger terrace ~ 6m x 25m. • No private on-plot parking. 	<ul style="list-style-type: none"> • Terraced in groups of 3 or more. • Secure rear access to all dwellings. • Retaining walls across contours to be avoided. • Bay fronted dwellings encouraged. 	<ul style="list-style-type: none"> • 2 storey buildings on branch roads. • 3 storey buildings concentrated on block edge facing green boulevard. 	<ul style="list-style-type: none"> • Up to 1m deep frontage for smaller terrace. • 2-3m frontage for larger terrace with low walled front boundary treatment. • Maintain a continuous building line.

The rigid rectangular block form indicated is for ease of illustration and applications will need to respond to site specific needs and to the character area being developed.



Blocks aligned parallel to contours.

Figure 3.44 Development Relationship to Contours



Section A: Split level dwellings can effectively respond to topography gradients.

Figure 3.45 Split Level Units

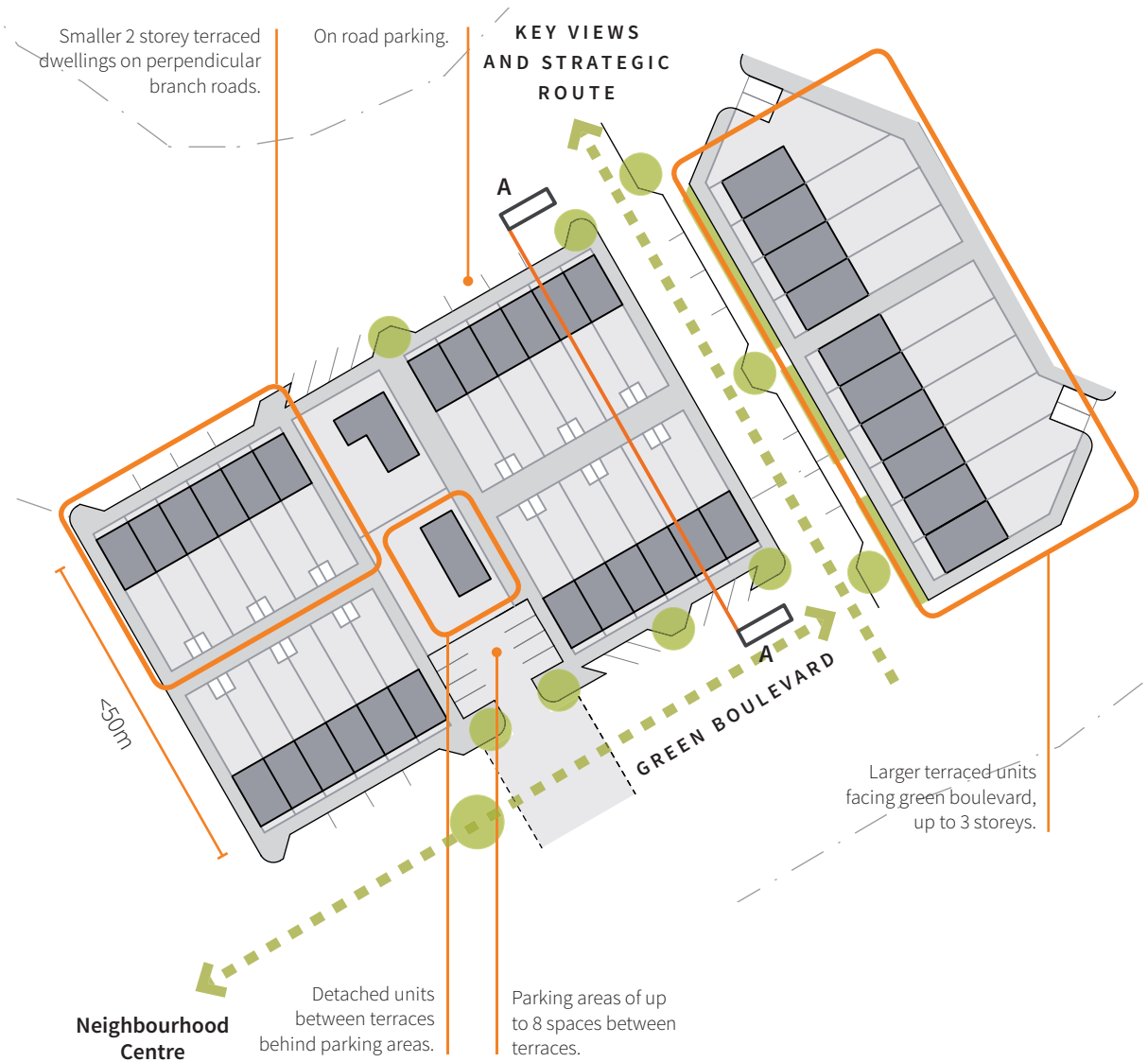


Figure 3.46 Example Block Plan
Orientation may vary according to site specific application

c2

North Eastern Area: Medium Density

The medium density residential development of the North Eastern Area has the following characteristics:

- Development blocks and building typologies responding to the topography of the site.
- Semi-detached properties encouraged throughout, reflecting principles of Centre and Edge.
- On plot parking.
- Open ended streets with views and connections to the site perimeter.



Figure 3.47 Key Area Plan



Figure 3.48 North Eastern Area Density Plan

Block Form	Plot	Typology	Height	Frontage
<ul style="list-style-type: none"> • 4 sided block. • Blocks oriented parallel to contours. • Linking roads of 1:12 maximum gradient. • Where applicable, secondary branch roads perpendicular to 	<ul style="list-style-type: none"> • Back to back plots only. • Approximately 10m x 30m. • On plot parking 	<ul style="list-style-type: none"> • Semi-detached units. • Rear alleyways not required (rear access to property maintained by side plot access). 	<ul style="list-style-type: none"> • 2 storey development throughout. • Building heights reflecting topography. 	<ul style="list-style-type: none"> • Minimum 5m deep private frontage with side driveway and access. • Height of plot/street boundary treatment up to 1m • Planted, built or no street boundary treatment if preferred.

The rigid rectangular block form indicated is for ease of illustration and applications will need to respond to site specific needs and to the character area being developed.

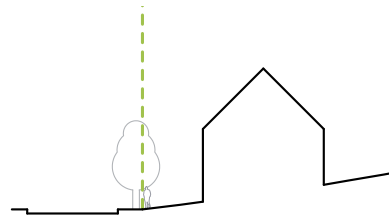
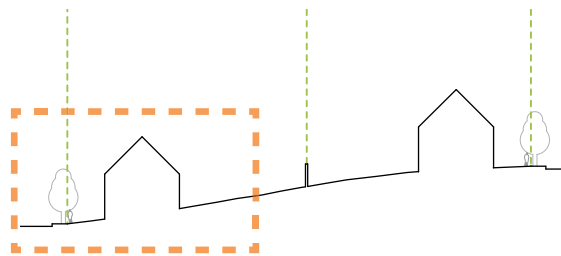


Figure 3.49 Typical Plot Frontage



Section A: Level change accommodated across the plot.

Figure 3.50 Unit Relationship to Gradient

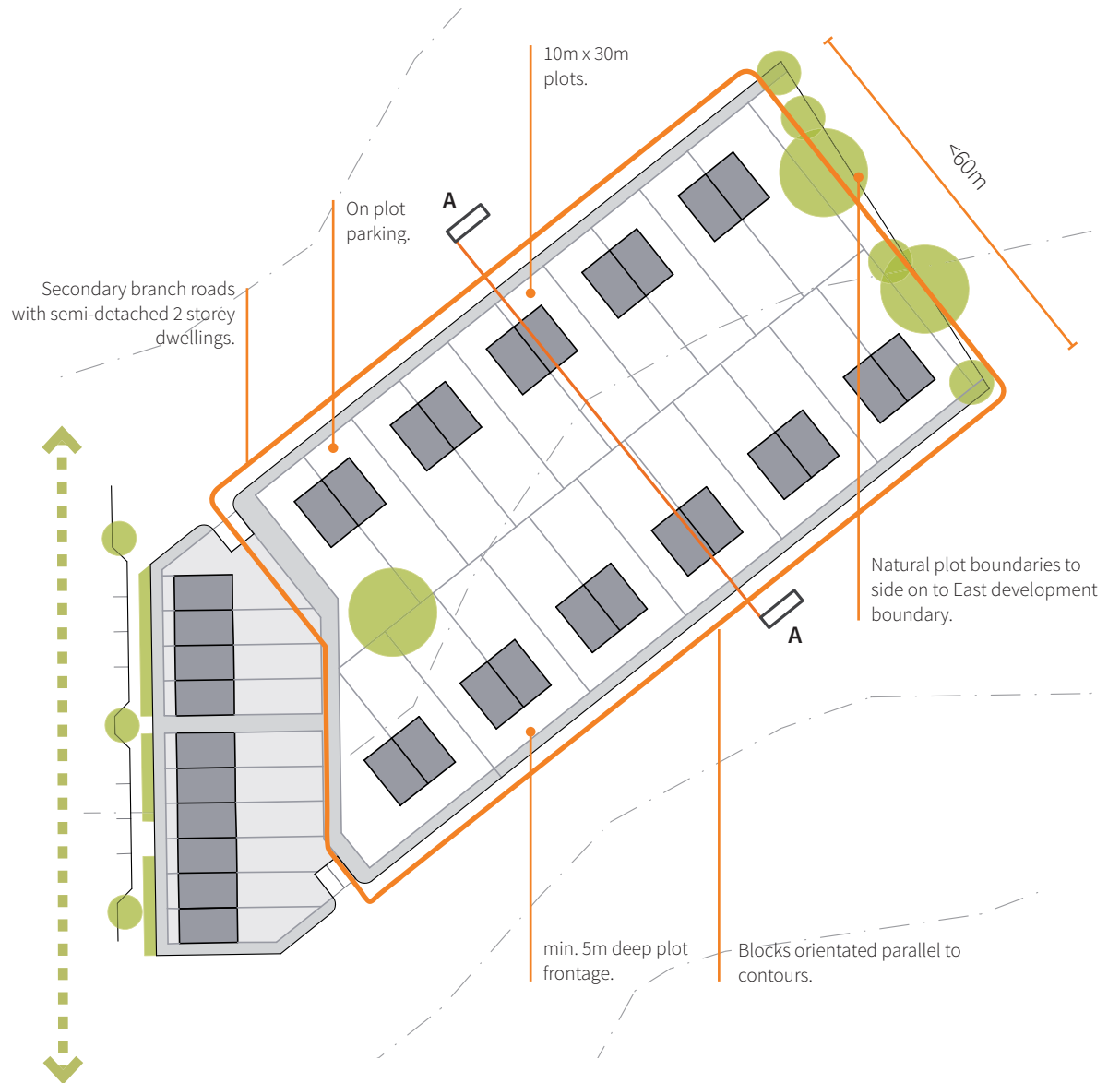


Figure 3.51 Example Block Plan
Orientation may vary according to site specific application

c2

North Eastern Area: Low Density

The low density residential development of the North Eastern Area has the following characteristics:

- Close proximity to the A361 dual carriageway.
- Sheltered by enhanced woodland and earth bunding.
- Driveways and dwellings accessed via tertiary access road.
- Dwellings facing perpendicular to A361 and contours.
- Existing hedgerows to edge of Uplowman Road serve as development/plot boundary.

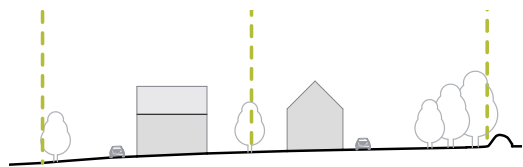


Figure 3.52 Key Area Plan



Figure 3.53 North Eastern Area Density Plan

Block Form	Plot	Typology	Height	Frontage
<ul style="list-style-type: none"> • 4 sided block. • Back to back plot depth to determine maximum length of longest side. • Shared surface driveway to settlement clusters. 	<ul style="list-style-type: none"> • Back to back plots only. • $\geq 15m \times 30m$. • Siting of properties on plots can vary across development. • Dwellings generally facing perpendicular to contours. 	<ul style="list-style-type: none"> • Typically smaller detached units throughout but sensitively designed higher density development may be considered. • Allows buildings to individually respond to topography. • Building form and orientation can vary per plot. 	<ul style="list-style-type: none"> • Typically 2 storey. • Heights of buildings can vary. 	<ul style="list-style-type: none"> • Building frontage to face perpendicular to contours and A361. • Properties within building groups to face driveways • Staggered building frontages acceptable emphasising informality. • Minimum 5m private frontage (side driveway and access).

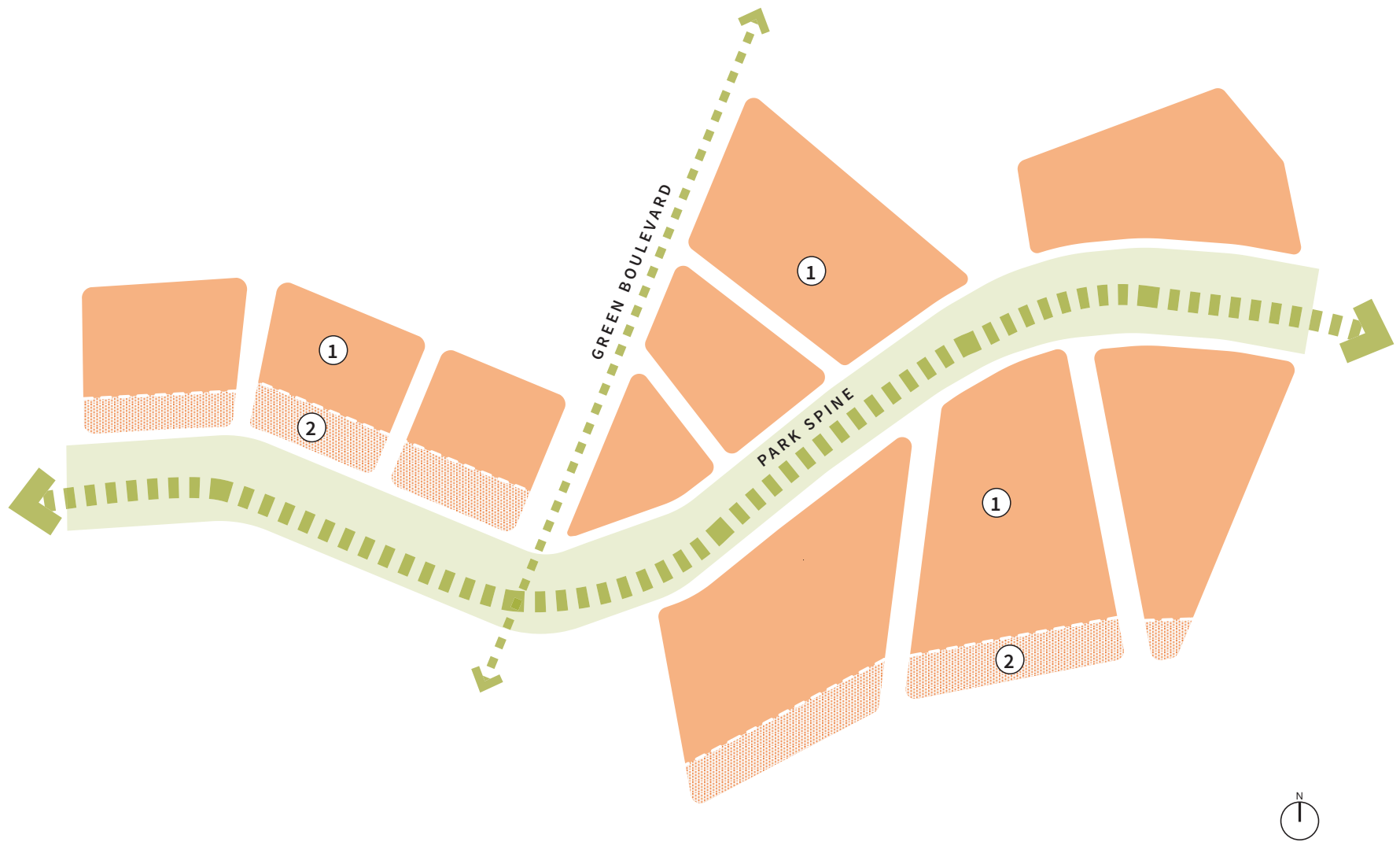


Section A: Dwellings can have different forms and orientations, and can be sited differently on plots. Natural plot boundary treatments are encouraged.

Figure 3.54 Typical Plot Section



Figure 3.55 Example Block Plan
Orientation may vary according to site specific application



- ① Parkland
- ② Park Edge

Figure 3.56 Parkland Key Area Plan

C3

Parkland

The Parkland development bridges a spine of green space that lies on a steep gradient. Green boulevards cross this spine, connecting Centre with Edge.

The Parkland Key Area Plan (Fig. 3.56) describes the attributes of this development area and correlates with the development boundaries identified in the Masterplan SPD.

The Park Edge is a unique treatment that has specific design guidance. Parkland development generally has a consistent relationship with the Park Spine which always has a higher density of development akin to green boulevards.



Figure 3.57 Key Area Plan

The principles of Centre and Edge are articulated across an individual block. Development blocks incorporate residential building typologies from terraces to detached plots with intermediate semi-detached dwellings back to back with an internal mews street. This consequently enables a mix of type and end user demographic, compatible with anticipated development phasing.



Figure 3.58 Key Area Plan

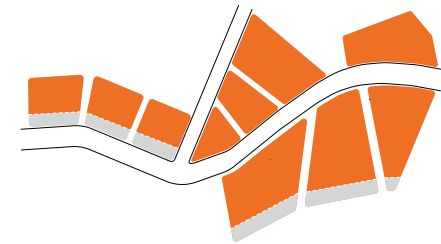


Figure 3.59 Parkland Density Plan

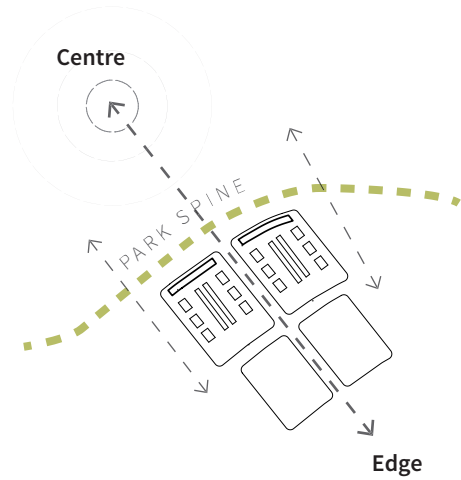
Parkland

The Parkland residential development area has the following characteristics:

- Closer proximity to canal tow-path, Sustrans cycle route and countryside walks.
- Natural identity partly informed by an existing 'spine' of mature planting.
- Intermediate land area between Blundells Road and West Manley Lane.
- Large development blocks enable internal mews street development.

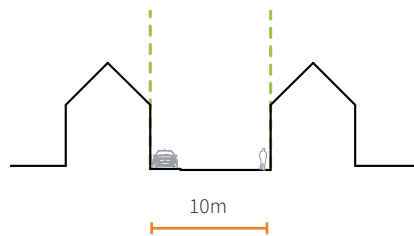
Block Form	Plot	Typology	Height	Frontage
<ul style="list-style-type: none"> • Larger irregular blocks constrained by existing hedgerows. • Block no greater than 110m on its shortest side. • Open ended inhabited mews streets acceptable to subdivide blocks. • Cul-de-sacs not acceptable. 	<ul style="list-style-type: none"> • Back to back plots only. • Terraces ~ 6m x 25m. • Semi-detached ~ 10m x 30m • Mews ~ 10m x 20m • Detached ~ 15m x 30m 	<ul style="list-style-type: none"> • Mix of large terraced, semi-detached, mews and detached. • Semi-detached units backed by internal mews. • Guided by centre to edge principles. • Bay fronted terraces encouraged throughout. 	<ul style="list-style-type: none"> • 2 - 3 storey. • 3 storey buildings acceptable along green boulevard and spine edge. 	<ul style="list-style-type: none"> • Mews dwellings facing directly on to lane. • Planted front boundary treatment to detached dwellings. • 2-3m deep threshold to terraced units.

The rigid rectangular block form indicated is for ease of illustration and applications will need to respond to site specific needs and to the character area being developed.



Strong connectivity to centre and edge continue to be reinforced in Parkland development

Figure 3.60 Centre to Edge Relationship



Section A: Properties facing directly on to mews street with no private plot frontage.

Figure 3.61 Mews Street Section



Mews dwellings (back-back with semi-detached plots) providing 1-2 bedroom units.

Detached dwellings remote from the green boulevard may be appropriate for Spine Edge.

Semi-detached properties on secondary branch roads.

Terraced properties up to 3 storeys facing spine edge or green boulevards (with servicing rear access).

Figure 3.62 Example Block Plan
Orientation may vary according to site specific application



Figure 3.63 Key Area Plan

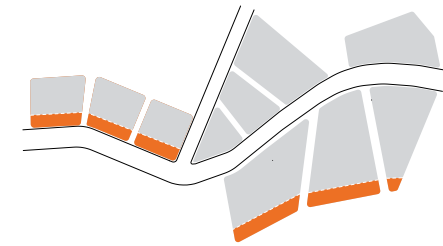


Figure 3.64 Parkland Density Plan

Parkland Edge

The Edge treatment of the Parkland residential development area has the following characteristics:

- Edge condition of development facing Manley Lane.
- Development in this scenario does not extend in to West Manley Lane character area
- South facing frontages throughout.
- Close proximity to canal tow-path, Sustrans cycle route and countryside walks.

Development at Parkland Edge is expected to be larger properties with a strong frontage to development but generous spacing between buildings. High quality self-build development that adheres to this principle is encouraged for the Parkland Edge.

Block Form	Plot	Typology	Height	Frontage
<ul style="list-style-type: none"> • Southerly edge treatment to Parkland block. 	<ul style="list-style-type: none"> • Back to back plots only. • ~ 20m x 40m • Outbuildings set back behind building line. • On plot parking. 	<ul style="list-style-type: none"> • Detached buildings set back from carriageway. • High quality self-build development is encouraged 	<ul style="list-style-type: none"> • 2 - 3 storey. 	<ul style="list-style-type: none"> • Planted boundary frontage. • Buildings to have strict consistent building line.

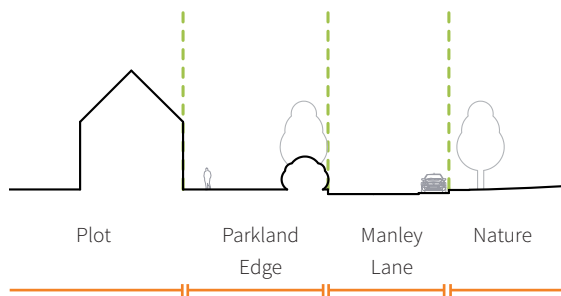


Figure 3.65 Typical Plot Section



Figure 3.66 Example Block Plan
Orientation may vary according to site specific application

C4

West Manley Lane

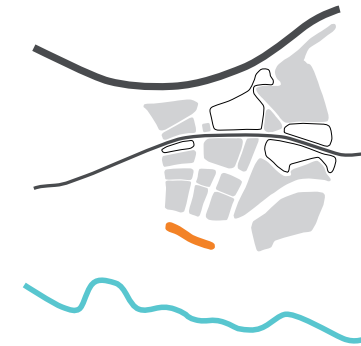
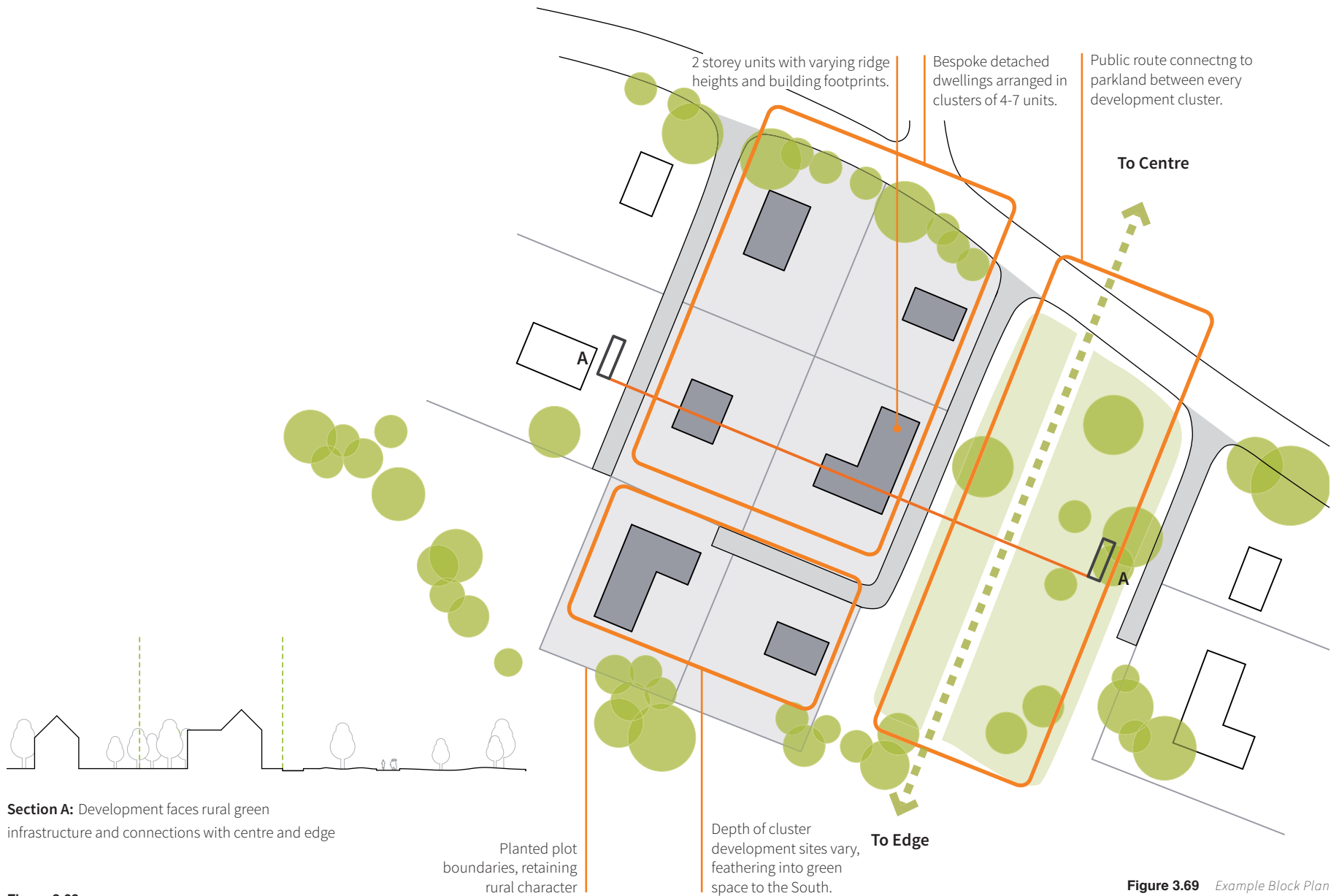


Figure 3.67 Key Area Plan

The West Manley Lane residential development area has the following characteristics:

- Informal dispersion of existing detached properties
- Building clusters akin to traditional agricultural groupings
- Mature hedgerows and banks define land boundaries
- Narrow lanes create a natural 'shared surface', no hierarchy of road user = mutual awareness
- Direct relationship with parkland and natural features

Block Form	Plot	Typology	Height	Frontage
<ul style="list-style-type: none"> • 4 sided. • Blocks may extend variably into the natural parkland to the South. 	<ul style="list-style-type: none"> • Back to back plots only -no access provided to sides or rear of detached properties • ~ 20m x 25m. • Rear access to plots not required. • Properties must not back onto surrounding public routes or spaces. • Planted property boundaries to rural edge 	<ul style="list-style-type: none"> • Bespoke detached units arranged in clusters of 4-7. • Building footprints are expected to vary within cluster. • Accessed via shared surface driveways. 	<ul style="list-style-type: none"> • Buildings no more than 2 storeys but ridge heights can vary among clusters. 	<ul style="list-style-type: none"> • Planted boundary frontage. • Buildings set back by a minimum of 3m.



Section A: Development faces rural green infrastructure and connections with centre and edge

Figure 3.68

Figure 3.69 Example Block Plan

Orientation may vary according to site specific application

Creating a Modern Lifestyle

d1 Mix of Uses & Activities

d2 Movement, Accessibility & Safety

d3 Parking & Servicing

Creating a Modern Lifestyle

There are many pressures and enjoyments of the modern lifestyle. The 21st century has brought with it unprecedented connectivity both physically and digitally and yet a mark of our society is a greater sense of individualism and independence from one another. The boundaries between work life and home life for many have become blurred bringing great flexibility on one hand but a compromised freedom on the other. The challenges of climate change have driven not just patterns of living but new industries and markets. Ever improving health care is resulting in greatly improved life expectancy which is impacting the economy and family life. Efficiencies in transport systems have meant the world is a smaller place but in the UK the effects of globalisation are felt ever more close to home with the consolidation of the manufacturing sector and the subsequent changes in our urban areas as a result.

All of these factors defining our modern lifestyle have an impact and an expression in the built environment. Often, the manifestation of these factors are seen in retrofit responses and solutions and reactive measures. This Design Guide promotes a pro-active approach to addressing the needs of today's lifestyles without prohibiting the possibilities of tomorrow's lifestyles. It asks designers to seek to accommodate new and emerging patterns of living in the two following areas:

Trends we seek to reverse or mitigate:

- Growing isolation of individuals
- Loss of community
- Reliance on the car
- Loss of local services & facilities

- Cluttered streets & buildings as a result of multiplying 'bolt ons'

Trends which can be harnessed and celebrated:

- Energy efficiency of buildings
- Micro-generation of electricity
- Improved efficiencies in district heat & energy capabilities
- Smart city application of technology
- Wider appreciation of the integration of human and natural habitats and the potential gains and value in doing so
- Fast broadband & wireless connectivity in private and public places
- Wider opportunities for flexible working

d1

Mix of Uses & Activities

The Masterplan SPD provides a spatial demonstration of the mix of uses specified in Local Plan Part 2. The Design Guide emphasises particular elements of this spatial expression and considers in slightly more detail how a functional and effective mix of uses should be included and how they can be sustained as part of the Urban Extension.

As with all other design principles within this guide, those governing the mix and location of various uses within the development relate to the philosophy of Centre and Edge reflecting the varying approach to the range of uses and activities defined in the Masterplan SPD.

Focus on Neighbourhood Centre

The Centre is the location which most suitably accommodates facilities which serve the whole community. Co-locating uses in one central place promotes their sustainability. Where a mix of uses are located near to one and other, activities can become complimentary, eg. school drop off and pick up combined with a visit to the shop or GP.

Creating a walkable neighbourhood.

The Centre lies at the most accessible location for the majority of the community. Much of the development lies within a walkable distance of the Neighbourhood Centre located along Blundells Road. It is essential that this accessibility is not frustrated by the layout of development and the expectation is of a permeable layout (in accordance with the masterplan structure) which

encourages pedestrian and cycle movement to and from the centre.

The mix of uses at the centre will vary from building to building but it will also benefit the diversity of activity to vary the uses vertically within buildings. This will provide different opportunities for elevational treatment.

While the main focus for uses other than residential is at the Neighbourhood Centre a small amount of work space in the form of live-work units can be included anywhere across the development. These should be considered as an integrated part of the residential environment and conform to the same guidance on character areas and groupings of buildings.

d2

Movement, Accessibility & Safety

Creating an inclusive and accessible environment for all begins with the consideration of the structure of the Masterplan SPD. The movement around the development stems from the inherent permeability created through a well thought out block structure. Blocks and streets should:

- Create direct routes to the Neighbourhood Centre
- Form a clear hierarchy which naturally signposts users towards their destination (primarily the Centre or the Edge of the development)
- Follow the block principles according to the character areas to define a hierarchy to the development but also distinguish the character between areas

Fundamental in the structure of the place is the open space hierarchy. This is hung on a framework of Green Infrastructure.

The Green Infrastructure supports the creation of an accessible environment by combining public realm, engineering and ecological facilities to create highly legible green boulevards & other green corridors extending from the Centre of the development to its Edge. In this way accessibility towards the rural setting of the place is ensured.

Appropriate provisions are made for disabled facilities throughout the development. When it comes to the public realm design, drawing any distinction and limiting or prescribing any fixed location for these facilities should be avoided. Instead the approach to the public realm should be to 'design for all' from the outset where the majority of car parking spaces are generously spaced and all public footpaths and streets are dimensioned to suit all users.

Designing for Gradients

The Masterplan SPD takes a strategic approach to the slopes found across the masterplan area. In some cases development is parallel to the contours to enable a certain gradient along the length of the street, in others the development is perpendicular to the contours to create a particular character. Generally the gradient for areas of public realm should not exceed 1:12. In certain situations and in close consultation with the Highways Authority this may increase to 1:10 or in very limited number of circumstances, 1:8. Care should be taken in all circumstances however to avoid:

- Significant stepping at the party wall interface where terraces cross the contours

- Major retaining structures which overly engineer the natural topography and create access and other building regulation issues within the buildings and between streets and spaces
- Layouts which result in houses which are individually served by steps or ramps in order to overcome gradient issues

Cycling and walking have been a primary consideration in the design of the Masterplan SPD and the detailed design of the development should maintain a focus on achieving pedestrian and cycle connectivity across the development, to the Neighbourhood Centre and more widely to and from the rest of Tiverton.

All environments from the Centre to the Edge should be pedestrian and cyclist friendly, promoting the free movement around, and to and from, the Urban Extension.

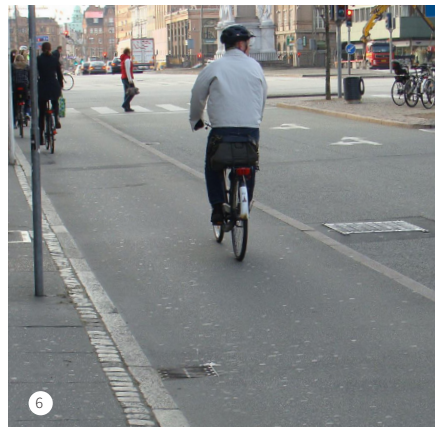
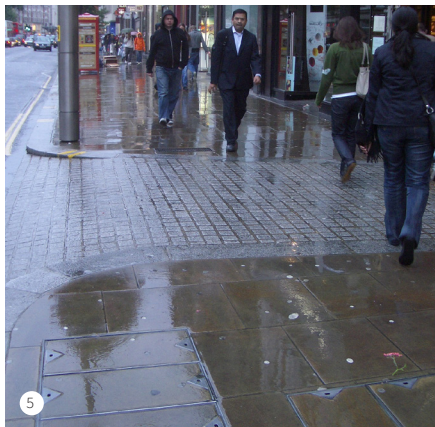
Secure by Design

The Masterplan SPD provides a strategic framework which ensures a safe and secure development emerges. Primarily this is achieved by creating a structure of streets which are overlooked by buildings. The Masterplan SPD minimises areas where surveillance of the streets is compromised or antisocial behaviour can go unnoticed. While not explicitly attributed to secure by design principles, many of the aspects of this design guide contribute to a safe and secure environment that seeks to design out crime.

Lifetime Homes/Flexible Design

Developments should be built to incorporate the principles of Lifetime Homes and which allow for flexible occupation over a lifetime. This places community at the heart of the development.

1. Street width and parking can affect the behaviour of street users without the need for traffic calming or signage
2. High quality materials are robust for all users and help the overall design continuity
3. Urban open space should be flexible and attractive to a variety of users
4. Street furniture, cycle parking and trees can be used strategically in the design of spaces and streets
5. Kerbs and carriageway levels can be used to promote varied priorities
6. Segregated cycle lanes are appropriate on primary transport corridors
7. Shared spaces used in residential streets can promote informality and the sense that the space is for more than vehicular movement -play and social space
8. Well considered public realm should specify high quality materials that do not discriminate against any user groups.



Parking and Servicing

The approach to parking across the development correlates to the overall structuring characteristics of Centre and Edge. Naturally the streets and spaces across the development contrast in character and size -those at the Centre and near to it have more formal and more closely grouped buildings; those at the edge are afforded more space and become more informally arranged. Different parking arrangements are more suitable for each of these locations.

The approach to parking should also be considered in the overall aspiration to create a highly legible and pedestrian friendly public realm. For this reason parking should never be the driver of the dimensions and arrangement of the street or space. Neither should it obstruct or clutter the street or detract from the overall objective of the street network which is to maintain permeability between the Centre and the Edge. The Eastern Urban Extension is a place where people can be expected to walk, whether to access local services and recreation or to access public transport and links to places of work and destinations

further away. While car ownership and use is acknowledged as a reasonable characteristic of life in Tiverton, the Local Plan and broader national planning policy aspire to reduce car use in order to address climate change globally.

Groups of parking spaces

Parking should be grouped appropriate to its type as outlined in the parking type guidance overleaf. Grouped parking should be considered alongside the arrangement and design of houses and their elevations. In particular there should be careful consideration of the width of the street in relation to the height of buildings, created by the grouping and type of parking either side of the road.

Parking Courtyards

In accordance with policy DM8, Local Plan Part 3, there will be no residential parking courtyards permitted. These are considered to create unattractive, potentially unsociable environments and fundamentally they undermine the activity on the street due to

people accessing properties through the rear entrance.

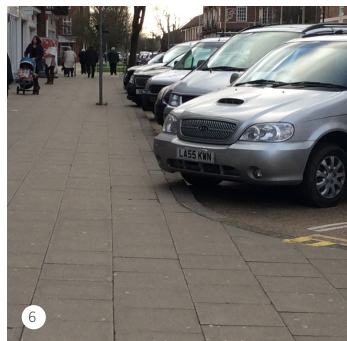
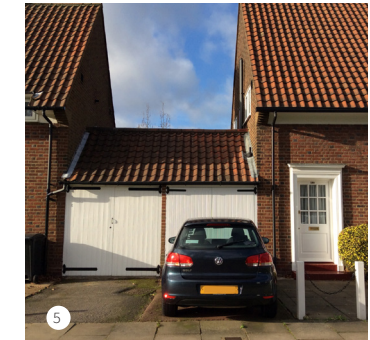
The exception to this will be in the case of the Neighbourhood Centre where a parking courtyard for retail, office and community use could provide a secure alternative to an external car park while limiting the amount of parking in the public realm in order to maintain the quality of the street and public space along Blundells Road.

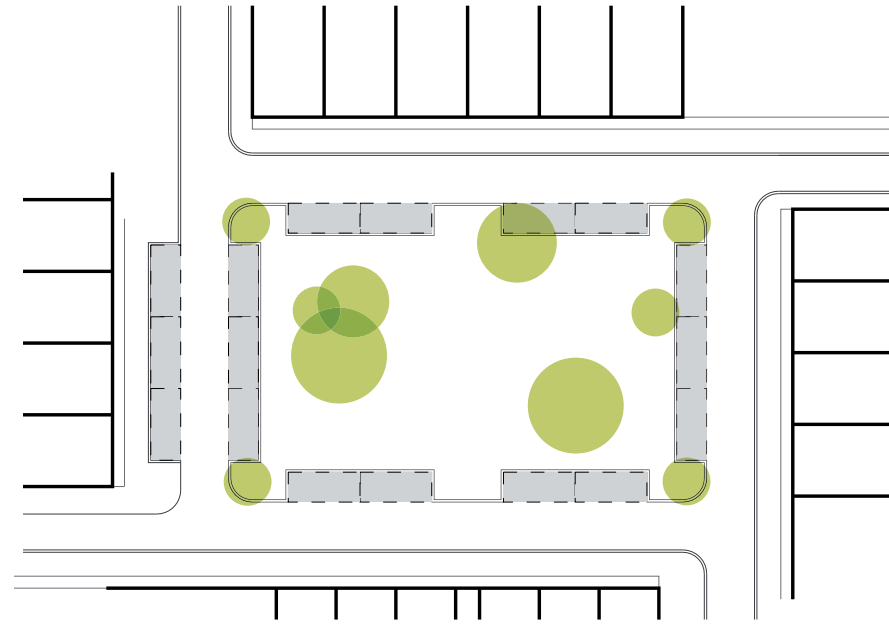
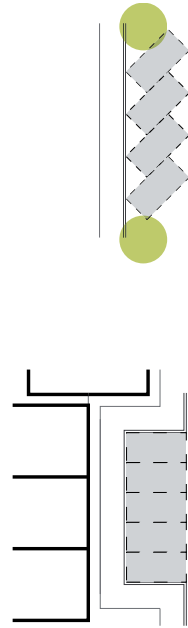
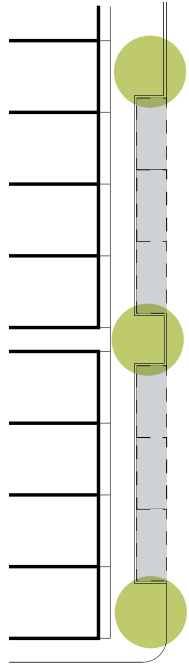
All parking provisions and allocations should be compliant with the MDDC Parking SPD. It should be noted that the provision of a garage is not considered a parking space.

Servicing

Servicing of employment areas should not dominate the environment and where possible should be provided to the rear, together with adequate turning provision.

1. Short stay parallel parking as part of shared surface environment
2. Parallel parking can perform a traffic calming function and create a separation between traffic and the public realm
3. Parallel parking in a residential situation
4. On plot residential parking
5. Attached garages maintain the continuity of a building line
6. Short stay eschelon parking in a Neighbourhood Centre situation
7. Parking spaces defined by trees and other features create an attractive street scene and enable pedestrian permeability
8. Providing small groups of parking spaces in one location can allow some areas of housing to be car free
9. The informality of a home zone environment encourages shared use and enjoyment of the street
10. Larger, detached houses can still achieve a continuous street frontage with the use of boundary walls and garages along a building line





Centre

Parallel Parking

Kerbside parking parallel to the axis of the pavement, bays may be marked or unmarked. Groups of up to 3 spaces, divided by trees or planting which also provide crossing places between parked cars.

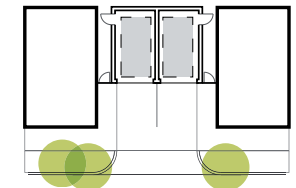
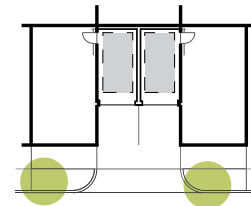
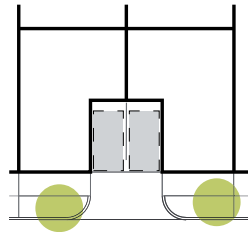
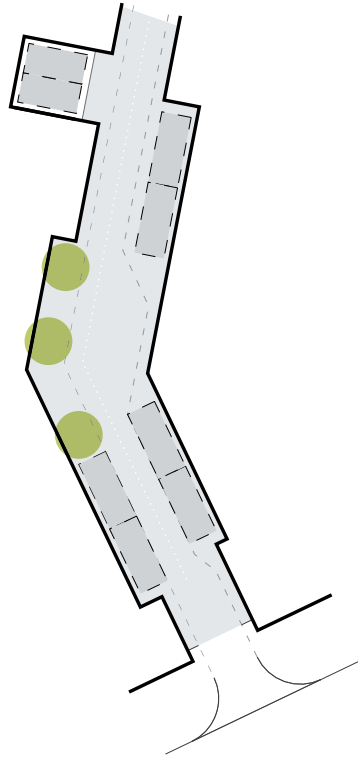
Eschelon & End-on parking

Eschelon: kerbside parking at less than right angles to axis of the pavement, generally in marked bays. Groups of up to 4 spaces, divided by tree planting.

End on: kerbside parking at right angles to axis of pavement, generally in marked bays. Coinciding with break in building line.

Parking around open space

Kerbside parking arranged around the perimeter of open spaces and green boulevards. This could be parallel, eschelon or end-on. Consideration should be given to maintaining access to the open space between parked cars.



Edge

Mews street parking

Parallel or on plot parking within mews forms part of the shared surface. Parking spaces purposefully protrude to create naturally traffic calmed spaces. No more than 2 spaces grouped together. Trees, planting and other street furniture used to mark bays.

On plot parking

Uncovered parking area adjacent to the side or front of the house. Can be hard paved or with a landscape material which allows grass to penetrate the surface.

Attached garages

Paired or individual garages to the side of house. Garage doors in line with the building line or slightly setback.

Detached garages

Paired or individual garages to the side of house. Benefits from an attached boundary to the house. May be in line or set back from building line.

Design Detailing

- e1 Built Form and Scale
- e2 Fenestration
- e3 Thresholds
- e4 Details
- e5 Materials
- e6 Statutory Guidance
- e7 A Place to be Proud of

e1

Built Form and Scale

The built form of the Tiverton EUE should:

- Acknowledge the historic vernacular of Mid Devon.
- Respond to the unique geographical constraints of (each area of) the site.
- Be a development that is of its time.
- Allow for variation between the development character areas.

As a consistent thread, the architectural expression of development should reinforce this Design Guide's principles of Centre and Edge:

Neighbourhood Centre

The Neighbourhood Centre should be clearly identifiable with a larger scale of buildings used, albeit suitable to the context of the setting and its location. Buildings of 3 storeys facing Blundells Road will serve to enclose and frame the EUE's central public realm.

Taking reference from local settlement centres, the Neighbourhood Centre should present itself as a formal terrace of buildings facing Blundells Road.

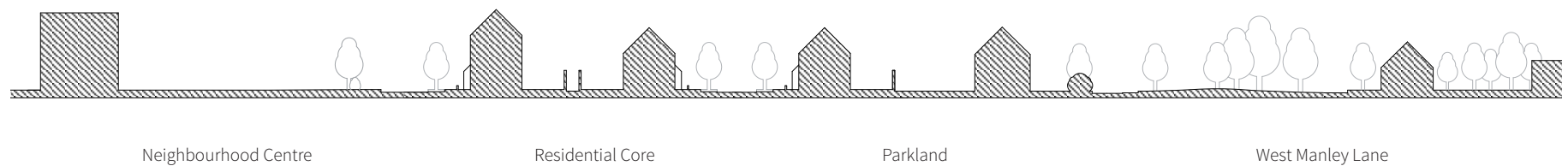
Green Boulevards

The buildings (typically residential) facing Green Boulevards should follow a similar principle of larger scale buildings, typically 3 storeys. The regularity/density of terraces on green boulevards will be less frequent in the transition from Centre to Edge with secondary branch streets breaking down the scale of development.

Bay fronted terraces are encouraged throughout.

Residential areas

Away from green boulevards, residential development should typically be 2 storey, pitched roof properties with bay frontages. A repetitive rhythm of facade treatment is encouraged



Centre

Edge

Southern Edge

At the southern edge of development, the scale of buildings will be lowest, with an allowance for single storey units. The form of development is expected to follow a traditional form with pitched and hipped roofs.

Variation of form between individual plots is encouraged at the south edge (whilst adhering to the parameters identified in the character areas). The southern edge of the Eastern Urban Extension development is appropriate for high quality self-build development opportunities.

Employment Area

Because of the unique function of the employment area within

the Eastern Urban Extension, its built form will also have unique attributes. As identified in the Employment structure chapter, larger scale commercial buildings are acceptable against the A361.

Generally, 2-3 storey buildings are appropriate across the employment area with development density becoming increasingly permeable towards the residential. Employment development becomes mixed use where the relationship with residential properties becomes more direct. Consequently, the form and scale should be complimentary to residential development thus allowing for flexible future use if required.

Roofs

Eave and ridge levels are welcomed to change across the site, responding to the topography and allowing for views between buildings. Where pitched and hipped roofs are employed, roofs are to be of a consistent pitch throughout a street.

Pitched roofs (including porches, unless flat) should be no lower than 35 degrees.

Fenestration

Achieving an adequately glazed principle facade ensures sufficient daylighting (and sunlight) to make appropriate internal environments and also, as importantly, suitably activating the public realm, contributing to the sense of place.

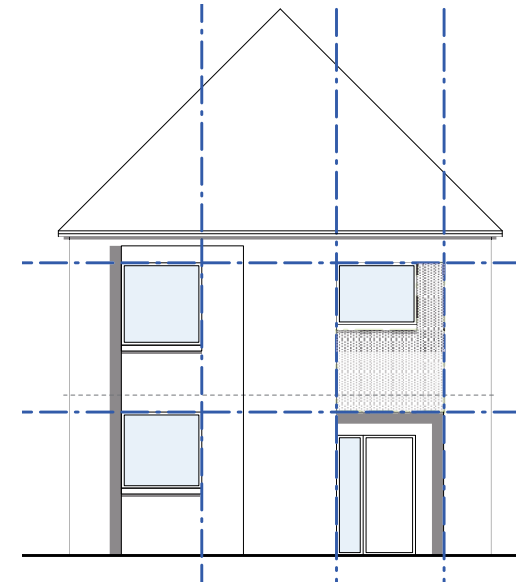
As well as articulating the use of buildings through their architecture and fenestration, the principle of Centre to Edge should also be articulated through the percentage of glazed facades.

The amount and format of glazing design should reflect a building's use and its location within the development.

Employment and public buildings should have the most significantly glazed elevations with entrances clearly articulated and welcoming.

Terraced residential properties (as identified through the residential character areas) should have bayed frontages with a consistent rhythm along the street, enhancing daylighting to properties and animating the street.

Dwellings (typically detached) toward the development edge are



Attention to proportion in facade design

expected to have a more introverted orientation toward private rear gardens. The proportion of the principle facade and its glazing should, however, not ignore the street and should instead contribute to the public environment irrespective of the siting of the building on a plot.

Throughout the preparation of design proposals, consideration should be given to the solar orientation of buildings with particular attention to glazing design. North facing principle facades should be adequately glazed and south facing facades should have adequate solar shading (without compromising generous daylighting to internal spaces).



Neighbourhood Centre



Live work units



Terraced street



Detached dwelling

Figure 3.70 *Examples of good facade design*

Facade design (for all buildings in the Eastern Urban Extension Masterplan SPD) should give consideration to the importance of proportion in the facade. This design guidance does not dictate the format of windows, nor should all windows match in height or width necessarily, but there should be a clear and complimentary relationship between the elements of a facade. A repeating rhythm of fenestration may provide coherence to the street scene. A vertical emphasis to fenestration is a local characteristic.

Thresholds

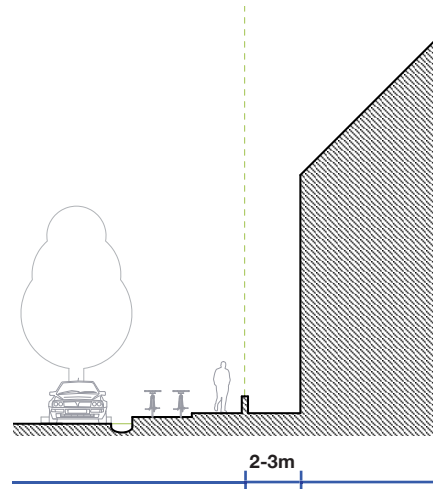
As identified through the character area guidance in this document, each building typology has specific frontage and threshold attributes. These respond to the identified street typologies, from Blundells Road and the green boulevards to the rural lanes.

Across the Masterplan development, the format of the boundary treatment is encouraged to change, from a low solid walling to planting.

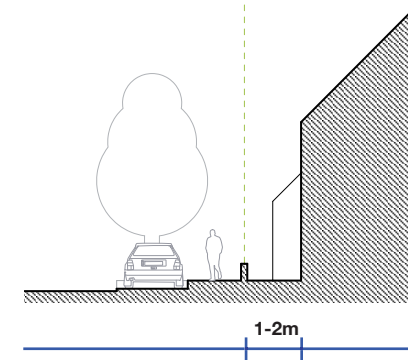
Reinforcing the principles of Centre and Edge, and ensuring an urban formality to the Residential Core and Green Boulevards, lawned frontages and fences or railings as a street threshold treatment are discouraged where the private frontage to a residential plot is less than 5 metres.

Planted boundary treatments are encouraged at all perimeters to the Masterplan development and throughout the West Manley Lane character area.

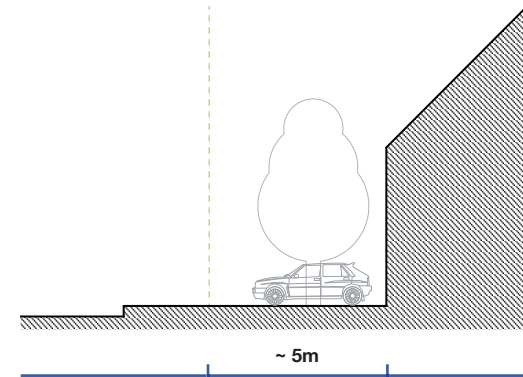
Applicants should note that level access into the entrance of dwellings will be required.



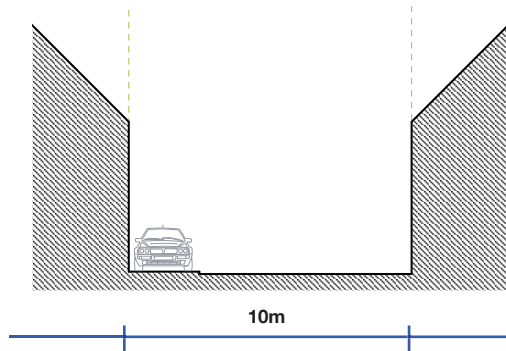
Larger terrace: Residential Core/
North Eastern Area/Parkland



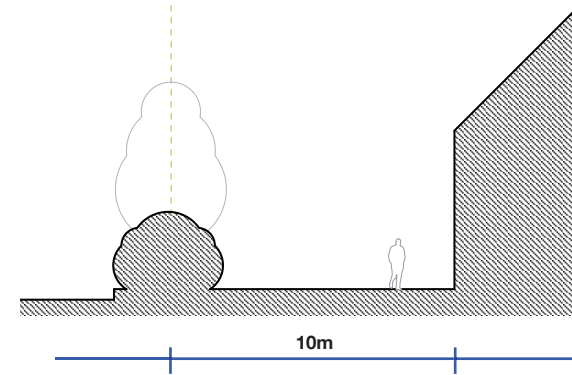
Smaller terrace: Residential Core/North Eastern Area



Semi-detached: Parkland/North Eastern Area

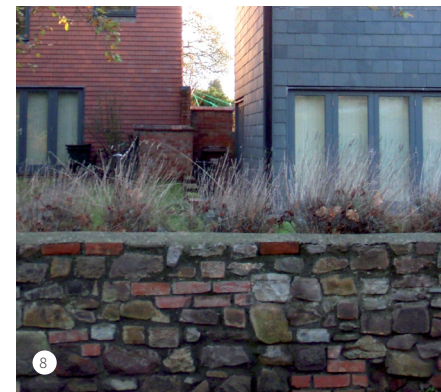
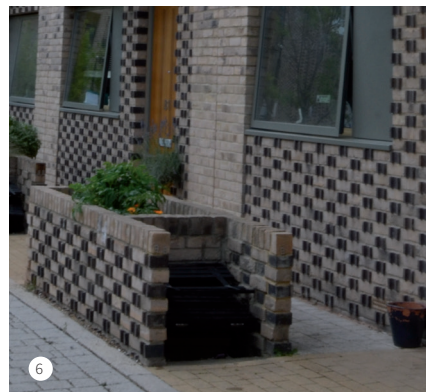


Mews: Parkland



Detached: North Eastern Area/Parkland Edge/
West Manley Lane

1. Planted border and ground surface material change
2. Formal, generous hard landscaped frontage with open railings
3. Lawned frontage to subtly denote private land - appropriate for Edge development
4. Low planted boundary treatment between properties
5. Low walled and planted boundary treatments - appropriate for Centre development
6. Low walled and planted boundary treatments - appropriate for Centre development
7. Porches can serve as an efficient boundary/threshold treatment
8. Low, natural stone walling is a traditional boundary treatment around Tiverton



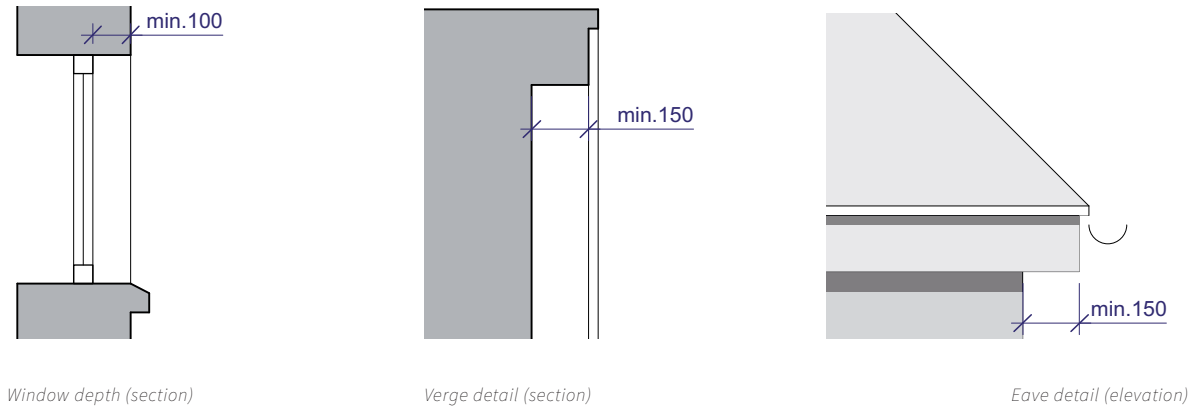


Figure 3.71 Architectural Details

Details

In parallel with sensitive fenestration and facade design, depth and shadow in buildings creates interest and mitigates against blank facades.

This can be achieved through various creative architectural treatments of the facade. This design guide does not, however, prescribe onerous architectural detailing throughout the EUE development. As a starting point, however, compliance with the following basic details is encouraged:

- Roof verge barge boards are to project from buildings by a minimum of 150mm.
- Pitches roof eaves are to project from building by a minimum of 150mm.

- Window and door frame reveals are to be a minimum depth of 100mm.

These recommendations are principally specific to residential development in the Masterplan SPD. Their inclusion endeavours to provide depth to facades and should be read in conjunction with the fenestration chapter.



Render finish



Multi-stock brickwork

Figure 3.72 Examples of material treatments

Materials

Materials and finishes that contribute to contextual development and local distinctiveness in and around Tiverton should be considered in planning applications for the Tiverton Eastern Urban Extension development.

Such attributes should be considered in the preparation of planning applications for sites across the development. A strict pastiche of historic vernacular should be avoided. A contemporary use of appropriate materials suitable for present day building techniques and technologies should be considered.

In parallel with this, a variation of facades (and the treatment of materials) across the development is encouraged, contributing to the identity of particular character areas rather than land ownerships.

In addition, the following recommendations, informed by the character analysis should also be considered:

- Where brickwork is employed, a multi-brick should be used that complements the traditional use of the area.
- Where a render finish is employed, a neutral colour should be used. A pure white should not be used and pastel colours should be avoided.
- Artificial cladding materials (eg. artificial timber cladding) should not be used. High quality artificial clay and slate roof tiles may be accepted.

These recommendations are not proposed to confine applicants to a strict material palette but are instead a means

of emphasising the grounded aspirations for quality and local distinctiveness across the development without dictating applicants' submissions.

Statutory Guidance

Space Standards

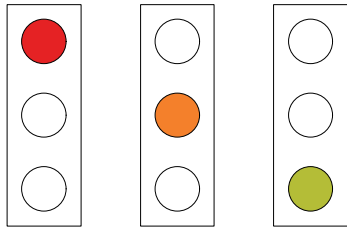
Mid Devon District Council planning authority will be applying the Nationally Described Space Standards to all residential development at the Eastern Urban Extension, as published by DCLG in March 2015.

The adjacent table shows the minimum space standards that all reserved matters planning applications are expected to comply with.

The size of gardens should respond to the size and scale of the property to which they are attached and its location within the development. The orientation of the garden may also influence its size. Careful consideration will be required to ensure the retention of existing trees and hedgerows do not become resented by future occupiers and create pressure for their removal.

Table 1 - Minimum gross internal floor areas and storage (m²)

Number of bedrooms(b)	Number of bed spaces (persons)	1 storey dwellings	2 storey dwellings	3 storey dwellings	Built-in storage
1b	1p	39 (37) ²			1.0
	2p	50	58		1.5
2b	3p	61	70		2.0
	4p	70	79		
3b	4p	74	84	90	2.5
	5p	86	93	99	
	6p	95	102	108	
4b	5p	90	97	103	3.0
	6p	99	106	112	
	7p	108	115	121	
	8p	117	124	130	
5b	6p	103	110	116	3.5
	7p	112	119	125	
	8p	121	128	134	
6b	7p	116	123	129	4.0
	8p	125	132	138	



Building for Life 12

All proposed developments should use Buildings for Life 12 (BfL12) as a design tool and should be supported by a BfL12 assessment. The local planning authority will use the traffic light assessment methodology to assist in the determination of applications. Schemes performing positively against the BfL12 criteria will be favoured.

An expectation will not be held by the local planning authority for the number of 'greens' achieved by a particular application, but applicants should endeavour to eliminate any 'reds' from applications.

Building Regulations

All reserved matters applications are expected to be compliant with the updated Building Regulations 2010. This is to include the updated requirements M4 and Q1 that came in to effect in October 2015.

Design Review Panel

Applicants are required to engage in the Design Review process.

Early engagement with the Design Review Panel and successive reviews provide the opportunity to gain independent advice on the design and its performance when measured against the Masterplan SPD and this Design Guide.

It is recommended that applicants participate in Design Reviews at 3 different stages in the design process:

1. At the laying out stage to define strategic objectives;
2. On embarking on elevation and street design;
3. At detailed design stage.

1. A successful mid-century terrace street
2. A successful contemporary terrace housing development and streetscene
3. Successful 'Edge' development can still be of a contemporary nature
4. Successful 'Centre' development can still have a relationship with the countryside



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A Place to be Proud of

As identified previously in the Character chapter of this Design Guidance, 'place' creates a valuable sense of identity.

The principle of Centre to Edge and the correlating development areas from Neighbourhood Centre to the edge at West Manley Lane provide the opportunity for area specific place-making within the Eastern Urban Extension development. The design guidance criteria identified for each of the development character areas endeavours to encourage this place-making and thus a sense of identity is striven for, distanced from the repetition of mass housing development.

Consequently, various building typologies are applied to the development areas in this design guidance. These building typologies are responsive to the masterplan design principles as explained in the typical block plans.

Attributes of existing examples of good place-making can be designed in to new development and should not be articulated with pastiche architectural ornament or stylisation. Instead, adopting good urban design principles of development (identified in this design guidance) and understanding the end user and all the associated activities and functions of a community will create a place to be proud of.

Whilst it is the structure of development that is the first priority, architecture and building typologies are not to be overlooked. The building typologies themselves contribute to the sense of place and are informed by local development as well as contemporary good design practices.

The design guide will aid applicants in honouring the principles of the Masterplan SPD and achieving the design and delivery of a desirable place that MDDC, applicants and citizens of Tiverton and the Eastern Urban Extension can all be proud of.

Summary

This Design Guide has been developed to assist developers and the local authority stakeholders in ensuring well considered design proposals for the development of the Tiverton Eastern Urban Extension.

Applicants are expected to familiarise themselves with all content within this Design Guide. For the benefit of all interested parties using this document, a summary is also enclosed enabling applicants, planning officers and council members to simply and easily ensure that applications can be scrutinised against the performance criteria explained here within.

<p>a Creating a Green Place Page 35</p> <p>A Green Network From the east principles of the Masterplan SPD the Urban Extension has been structured around the 8 key landscape features and constraints. The continuing design requirements should seek to respect and enhance the existing landscape network. To establish a structure of green links and spaces which forge a strong relationship between the development and the surrounding highly valued landscape.</p> <p>A Garden Neighbourhood The Design Guide reflects the aspiration to create a Garden Neighbourhood. Where possible, green routes are provided for the Design should demonstrate how a minimum design requirement environmental qualities are achieved with a minimum development quantity (also necessary to create a sustainable neighbourhood).</p> <p>Centre to Edge Across the Design Guide, reference to the centre and the edge are explicit. Creating a green place relies on establishing strong links between the Neighbourhood Centre and the surrounding edge. These links are physical as well as visual and ensure the landscape character of the area is prevalent throughout the development as the landscape is drawn in from the edge to the centre.</p>	<p>o Establishing the Structure of the Place Page 43</p> <p>Neighbourhood Centre Page 46 The Neighbourhood Centre provides a mix of building uses which are gathered around a public open space.</p> <p>The public realm at the Neighbourhood Centre (open space and street design) is more about people's activity (shopping, school, other facilities) and use by pedestrians, than vehicle circulation, access and parking.</p> <p>Entrances and foreways are clear and building elevations contribute to the quality and activity of the public realm.</p> <p>The NHS Site Page 54 Development is positioned and scaled to create a landmark when approaching along Blundells Road.</p> <p>The site layout creates a gateway and aids through movement to and from the North Eastern Area.</p> <p>Employment Area Page 56 All attempts are made to maintain street quality and buildings which front onto the street despite the likelihood of large format buildings.</p>	<p>Services and vehicle circulation is dealt with behind buildings keeping the street as an attractive place. <input type="checkbox"/></p> <p>Blundells Road Corridor Page 62 The design along the corridor establishes a sequence of treatments which culminate at the Neighbourhood Centre where vehicular movement gives way to the activity and quality of the public realm. <input type="checkbox"/></p> <p>Edge Relationships Page 64 The Design takes in to account the different edge conditions of the development and addresses these accordingly. <input type="checkbox"/></p> <p>Streets Page 68 The layout establishes a clear hierarchy of streets which combine suitable highway, public realm and building typologies. The range of streets demonstrate the transition from centre to edge. <input type="checkbox"/></p> <p>Green Infrastructure Hierarchy Page 74 The Design contributes to a diversity of green space across the development as a whole and demonstrates links between different scales and uses of green, public and open spaces. <input type="checkbox"/></p>
<p>c Creating a New Residential Community Page 80</p> <p>Residential Core Page 80 The block form is regular and 4 sided. They have their shortest dimension aligned to the green boulevard. <input type="checkbox"/></p> <p>Green Boulevards make direct and visual connections to the Neighbourhood Centre and serve as a multitude of functions including open space, sustainable drainage, pedestrian and vehicular movement. Streets generally conform to a grid arrangement with no cul-de-sacs or dead end roads. <input type="checkbox"/></p> <p>Houses are terraced of a variety of sizes and buildings are at least 2 storeys with 3 storeys concentrated along green boulevards. <input type="checkbox"/></p> <p>Parking is mainly on street, parallel or on-chassis and in some places grouped. There are no parking coverages. <input type="checkbox"/></p> <p>Houses are within 1.3m of the pavement providing small front gardens or setbacks resulting in a formal public realm. <input type="checkbox"/></p>	<p>North Eastern Area Page 83 There are 3 distinct density variations from the centre of the area (where access is through the NHS site) to the edges which in some cases have a back to back relationship with existing properties. The 3 areas demonstrate differences in house type, plot and street arrangement and public realm design.</p> <p>The green boulevards extending through the site establish a strong connection with Blundells Road through the NHS site (running SW-NE) as well as make the most of views and landscape connections to the north of Tiverton (running SE-W). The contrast of the route along the contours and the route across the contours is reflected in the design of the public realm and the architecture.</p> <p>Parkland Page 91 Block form is generally larger to allow a variety of building typologies, including some narrow streets within blocks. <input type="checkbox"/></p> <p>Green boulevards and the park spine are fronted by terraced properties which can be 3 storeys. <input type="checkbox"/></p>	<p>Properties fronting the park (denoted by the 'park edge' character distinction) are detached houses arranged on more generous plots. <input type="checkbox"/></p> <p>West Manley Lane Page 96 Buildings are arranged in small clusters similar to the groups of buildings found in rural locations. <input type="checkbox"/></p> <p>Clusters are divided by green routes which link the development to the park and wider countryside. <input type="checkbox"/></p> <p>Houses are no more than 2 storeys and are generally detached and bungalow in design. <input type="checkbox"/></p> <p>Generous plots allow greater setbacks and a variety of building orientation on the plot helps the buildings sit within the landscape having the effect of feathering the development towards the settlement edge. <input type="checkbox"/></p> <p>Access remains from the front of properties with buildings having a back to back relationship to help security and legibility. <input type="checkbox"/></p>
<p>d Creating a Modern Lifestyle Page 99</p> <p>Mix of Uses & Activities Page 102 Retail units, community services and office space is reserved for inclusion in the Neighbourhood Centre which is the focus of the mix of uses for the whole urban extension. The exception is the inclusion of some live-work units within the residential areas. <input type="checkbox"/></p> <p>The design of each individual area is considerate of the overall approach to a range of activities across the development. <input type="checkbox"/></p> <p>Design and layout facilitates good links to and from the Neighbourhood Centre as the focus of the mix of uses for the whole urban extension. This includes creating very legible visual links from the edge to the centre. <input type="checkbox"/></p> <p>Movement, Accessibility & Safety Page 103 Overall the design takes an inclusive approach where all aspects of the development are 'designed for all', including the provision of lifeline routes. <input type="checkbox"/></p> <p>The design creates direct routes to the Neighbourhood Centre making integral use of the green infrastructure. <input type="checkbox"/></p>	<p>There is a clear hierarchy which naturally signposts users towards their destination (primarily the Centre or the Edge of the development). <input type="checkbox"/></p> <p>The design deals with gradients appropriately while also ensuring the development creates identity from its setting. <input type="checkbox"/></p> <p>The design carefully considers how threats to community and personal safety and security can be minimised. <input type="checkbox"/></p> <p>Parking & Servicing Page 106 The design considers appropriate parking solutions according to character areas. <input type="checkbox"/></p> <p>The formality and variety of on-street (more urban) to on-plot (more rural) corresponds to the transitioning of development from the centre to the edge. <input type="checkbox"/></p> <p>Overall parking and servicing solutions are considered within the overall design of street and open space environments, not just as an after thought to meet a quota. <input type="checkbox"/></p>	<p>e Design Detailing Page 111</p> <p>Local Character Page 112 Design proposals reference the local character studies (Design Guide and Masterplan SPD appendix). <input type="checkbox"/></p> <p>Built Form & Scale Page 112 An increase in scale of buildings at the Neighbourhood Centre and along the green boulevards helps the legibility of the centre and routes to the centre. <input type="checkbox"/></p> <p>There is a recognisable approach to changing form and scale between residential character areas. <input type="checkbox"/></p> <p>Fenestration, Detail & Materials Page 114-119 The design employs a clear strategy for the proportioning and architectural treatment of buildings. Detailed design helps articulate the wider Masterplan SPD structure eg corner and other important buildings (use or position). <input type="checkbox"/></p> <p>Thresholds Page 116 Threshold treatments are applied to assist in the definition of character areas and to reinforce either a centre or edge location. <input type="checkbox"/></p>